

Louisburg Planning Commission Regular Meeting
6:30 P.M. July 26, 2023
City Council Meeting Room
215 South Broadway
AGENDA

Livestream link:

<https://boxcast.tv/view/louisburg-planning-commission-nyhzuyj5oboyg8yltqei>

Item 1: ROLL CALL:

Item 2: PLEDGE OF ALLEGIANCE

Item 3: ADOPTION OF THE AGENDA:

Item 4: APPROVAL OF THE MINUTES:

- Minutes from the June 28, 2023, Meeting

Item 5: PUBLIC COMMENTS:

Persons who wish to address the Planning Commission regarding items not on the agenda may do so at this time. Speakers will be limited to three (3) minutes. Any presentation is for informational purposes only.

PUBLIC HEARING BUSINESS ITEMS:

Item 6: 23002-SUP (Special Use Permit) and Site Plan for Scooters Coffee.

Item 7: 23001-Z (Rezoning) Zoning change, 7 West Amity Street from R-1 to C-3.

NON-PUBLIC HEARING BUSINESS ITEMS:

NEW BUSINESS:

Item 8: Presentation of proposed plot plan by scheduled visitor Joel LaJoie.

OLD BUSINESS: Any old business the Commission may wish to discuss.

Item 9: Annual Review of part 2: Comprehensive Plan/Community Vision and Land Use Safety page 101.

Item 10: Update from wayfinding sign committee.

Item 11: ADJOURNMENT:



LOUISBURG PLANNING COMMISSION MEETING MINUTES

Wednesday June 28, 2023

The Planning Commission of the City of Louisburg, Kansas met at 6:30 p.m. in the City Hall Council Chambers with Chairperson Thorvald McKiernan presiding.

ATTENDANCE:

Commission Members: Chris Hoffman, Carol Aust, Michelle Olson, and Michael Sharp
Mayor: Donna Cook
City Council: TJ Williams and Tiffany Ellison
City Administrator: Nathan Law
City Staff: Jean Carder
Recording Secretary: Robert Lake
Visitors: Doug Bohi, Brian Smith, and Joel Lejoie.

Item 1: ROLL CALL

Item 2: PLEDGE OF ALLEGIANCE

Item 3: ADOPTION OF THE AGENDA:

Michelle Olson moved to adopt the agenda, motion was seconded by Chris Hoffman, and passed 5-0

Item 4: APPROVAL OF THE MINUTES:

Michael Sharp moved to approve the minutes from the May 31, 2023 meeting. The motion was seconded by Michelle Olson and passed 5-0.

Item 5: PUBLIC COMMENTS:

Brian Smith and Joel Lejoie addressed the commission and advised they have plans to purchase the 200-acre plat of land previously known as the Louisburg golf course. They are wanting to develop this land for residential use. In approaching the Planning Commission, they were wanting to get their input as well as let them know of their development plan before they began.

Their plan is to have several different residential lots. Some would be estate size, villas, and townhomes. In developing the residential area, they spoke of utilities and where hook-ups would be for city utilities.

Discussion occurred.

Smith said the biggest challenge would be the sewer system. Smith stated there is a sewer system on the property that could possibly feed all the residents. City Administrator Law said that sewer system is operated through Maim County to serve the existing homes. Chairperson McKiernan asked where the water would come from Smith said there should be water already there. Staff said the property is served by a rural water district.

Lajoie asked the Commission if three-acre lots is the minimum for septic systems. Staff advised that was the minimum for septic through the state of Kansas and Miami County guidelines that was the minimum for septic. Lajoie asked if they were opposed to sand filtration septic systems. Chairperson McKiernan advised he would be opposed to any septic system and would prefer to see the development utilize city utilities.

Smith asked again what the land was zoned as City staff confirmed it is zoned as “R-1” Single-Family Dwelling District.

PUBLIC HEARING BUSINESS ITEMS:

Item 6: None

NON-PUBLIC HEARING BUSINESS ITEMS:

NEW BUSINESS

Item 7: Annual Review of part 2: Comprehensive Plan/Community Vision and Land Use Safety page 101.

Chairperson McKiernan asked for this item to be moved and discussed at the July 26, 2023 meeting. All members present agreed.

OLD BUSINESS:

Item 8: Scheduled speaker Doug Bohi on behalf of the Louisburg Landing project.

Doug Bohi approached the Planning Commission and advised he represents the company building the Louisburg Landing project. Bohi said he reached out to city staff to discuss the amount of rock in the area that is making it financially unfeasible to continue with basements. He would like to alter the plan for all units to have basements and allow for some of the plexes to be on slabs. He said staff asked him to come before the Planning Commission with the request. Bohi said he believes that he can get some basements in but may not be able to do all buildings due to the rock.

Carol Aust asked what he would accommodate the loss of space. Bohi said they would do nothing, and that he preferred basements, but the units are good sized units being two and three bedroom and they are not changing footprint of the residence.

Chairperson McKiernan asked when they would begin building, Bohi said within the next two weeks they would begin to dig test holes on the remaining units and see if they can get the basement in. Chairperson McKiernan advised the board did not have any material to review and he does not

feel like that he can give an answer at this time. Bohi said they were not a planned development and just zoned as such for that project. Chairperson McKiernan asked if they had a conditional use permit for the project, and if that was the case, they would need to go back through and review it. Bohi asked what type of review the Planning Commission needed the City Administrator advised that is why we requested Bohi to come speak to the Planning Commission about. Chairperson McKiernan advised he can talk, but they don't have any information in front of them to decide anything.

Bohi asked what the Planning Commission needed to review. Chairperson McKiernan stated a need to review the use permits to see what was required and if they make a change to something that they shouldn't without reviewing documentation. Discussion occurred.

Bohi said he will work with staff regarding what documentation he needs. Chairperson McKiernan advised there could be a special meeting held to discuss this project if needed.

Item 9: Discussion of wayfinding signs.

Chairperson McKiernan asked city staff when a good time would be to begin meeting with the subcommittee regarding wayfinding signs. City staff explained certain times and dates for using the conference room. City staff asked if there were any questions regarding the KDOT response. Discussion occurred for location of signage, type of signage, and costs.

Item 10: Building and Zoning report.

City staff provided the Planning Commission a compiled report of items. On this list were new residential builds, commercial builds, City projects, engineering projects, current list of lots available in subdivisions and empty businesses throughout the City of Louisburg.

Discussion occurred what could be utilized to obtain more business within the City of Louisburg.

Discussion of the zoning regulations on feathered flags occurred. The regulations states "Each business may display a maximum of two (2) flags per roadside with exception of a multi-tenant complex." The Planning Commission stated the regulations should have stated two (2) flags per business. The Planning Commission advised they will wait to change the wording of this until there is a different text amendment, to add this section as well.

Carol Aust asked about some areas in town if they have been notified that they need to mow their grass and if the two residences on South 16th Street had been provided letters. City staff provided information and update on that enforcement actions occurred.

Item 11: ADJOURNMENT:

Chris Hoffman moved to adjourn the meeting, seconded by Michelle Olson passed 5-0. The meeting adjourned at 7:29 p.m.

Submitted by Robert Lake

Item #6
23002-SUP (Special Use Permit)
Scooters Drive Thru Coffee Kiosk.
1500 West Amity Parcel ID: 1093003001004010

Owner: DML Properties, LLC
Applicant: Phelps Engineering, Inc.
Meeting Date: July 26, 2023

The owner of the existing parcel has agreed to allow for the current carwash building to be demolished and for the applicant to construct a new building to house a Scooters Drive Thru Coffee Kiosk on the property. The owner of DML Properties is asking for a Special Use Permit to allow the business to be operational on the same plat of land as the existing business without splitting the parcel. The Scooters Drive Thru Coffee Kiosk would utilize a total of 8 parking stalls adding one ADA parking stall. The number of current parking stalls for all businesses on the parcel is 49. The construction of the new building would decrease the number of parking stalls to 41, which still meets parking requirements.

In accordance with Section 705 of the City Zoning Regulations the need for required spaces for the current businesses located meet the needed minimum which means the new and current business would not need a variance for parking stalls. See below excerpt from the Zoning Regulations:

“Restaurants, private clubs and taverns: One parking space for each 2.5 seats based on the maximum designed seating capacity; provided, however, that drive-in restaurants shall have a minimum of at least ten parking spaces.”

“All other business and commercial establishments not specified above: One parking space for each three hundred (300) square feet of floor area.”

Burger King has a seated occupancy of 68. Based on the requirement of 1 stall per 2.5 seats a total of 27 parking stalls would be needed based on this requirement. The convenience store parking spaces are based on the square footage of the business. The square footage of the convenience store is approximately 1,974 square feet. Regulations require one parking stall per 300 square feet. With 1 parking stall per 300 square feet, the convenience store requires 7 parking stalls. This allows 7 additional parking spaces for employees.

Adjacent Property Zoning:

North – “C-3” General Business District (Multiplicity).

South – “C-3” General Business District (Amaco)/ “A-L” Agricultural District (Sutherland Lumber & Home).

East – “C-3” General Business District (Price Chopper).

West – “C-3” General Business District (Miami County Medical Center).

The City has provided written notice of this public hearing to all property owners within the required 200-foot radius of the subject property. Staff has also placed a public notice in newspaper to advertise this hearing. This ad appeared in the newspaper on July 5, 2023. To date Staff has not received any comments from the neighbors concerning this proposal.

All Planning Commission recommendations will be forwarded to the City Council for additional review and consideration. This item with Planning Commission recommendation will be presented to the City Council on August 21, 2023. Property owners within 200 feet of subject property described above, who would like to protest the proposed SUP will have fourteen (14) days after the Planning Commission makes its recommendation to submit a protest petition to the office of the City of Louisburg City Clerk. If a valid protest petition is filed with the City Clerk prior to the deadline, the change shall require approval by a 3/4 majority vote of the City Council. Protest petitions are available in the office of the Codes Administrator for the City of Louisburg.

NOTE:

The City of Louisburg Zoning Regulations does not specifically address a separate business located on a lot of record without the splitting of the lot. Within the City of Louisburg, there are several strip malls that have different owners of record on the same parcel.

With the construction of the Scooters Drive-Thru Coffee Kiosk, the vacant carwash will be demolished for the usage of the drive-thru lane for the business. They will also utilize a section of green space located north of the business for the addition of a drive-thru lane.



Proposed Scooters
Coffee Location

Eight Golden Factors to Consider

In 1978, the Supreme Court of Kansas handed down a decision in the Golden vs. City of Overland Park case that established the basis for considering and reaching a decision on zoning changes. The decision should be made based on the evidence submitted and the factors considered. The eight Golden Factors emerged from this case and have become institutionalized in the consideration of zoning changes in Kansas. A more recent court case determined that Special Use Permits were, in reality, a land use change and should be considered by the same factors.

The factors must be considered by the Planning Commission and Governing Body in order to determine whether granting or denying a zoning change, including a Special Use Permit, is reasonable. It is not necessary that findings on all the factors be favorable to approve or be unfavorable to deny the zoning change or Special Use Permit. Also, not all the factors carry the same weight and the weight may vary from case to case.

1. **The character of the neighborhood:** Factual description of the application area and surrounding property as to land uses, density, intensity, general condition, age of structure, etc.

2. **The zoning and uses of property nearby:** Factual listing of the zoning surrounding the property along with the specific abutting uses.

3. **The suitability of the property for the uses to which it has been restricted under its existing zoning:** How is the property currently zoned and what uses are allowed on the property? Are these uses suitable given surrounding zoning and site criteria? Are the current allowed uses the only ones which might be appropriate for this property?

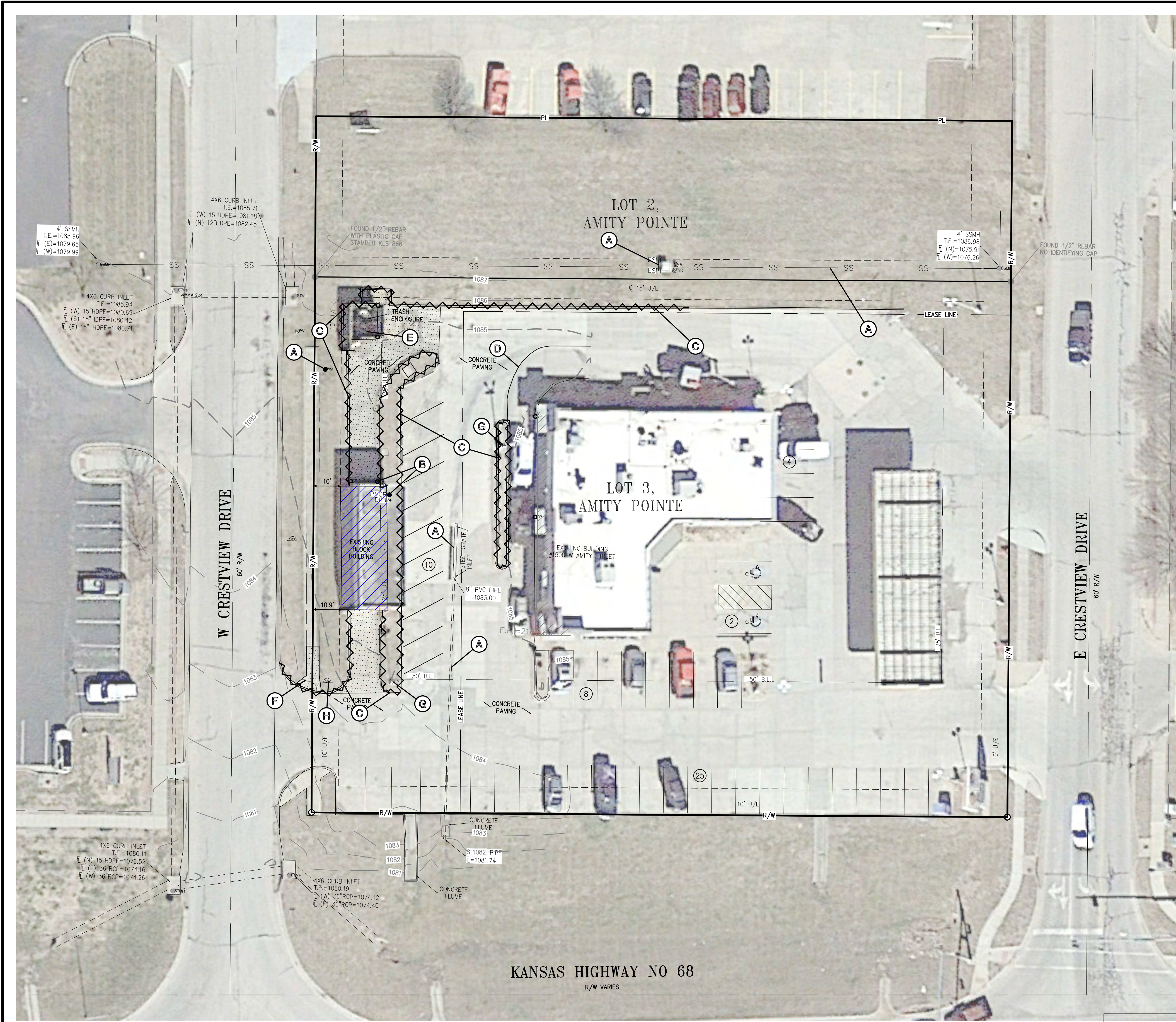
4. **Extent to which removal of the restrictions will detrimentally affect nearby property:** Can the uses allowed in the requested district be good neighbors to existing development? This is a subjective question. The focus should be on facts, not fears, and should be based on issues that zoning can address (e.g., allowed uses, minimum lot sizes, height, setbacks, traffic, etc.)

5. **Length of time of any vacancy of the property:** Factual information, but its importance can be somewhat subjective. A property might be vacant because the current zoning is unsuitable, but there may be other reasons not related to zoning. Some examples might be a glut of available property of the same zoning district, financing problems, speculation, lack of available services or other development problems.

6. **Relative gain to the public health, safety and welfare by destruction of value of the applicant's property as compared to the hardship on other individual landowners:** The protection of the public health, safety and welfare is the basis for zoning. The relationship between the property owner's right to use and obtain value from their property and the City's responsibility to its citizens should be weighed.

7. **Recommendation of professional staff:** Should be based on the evidence presented, the factors, adopted plans and policies, and other technical reports (e.g., Capital Improvement Programs, facility master plans, etc.) which speak to the topic and staff's best professional judgment.

8. Conformance with the Comprehensive Plan: Does the request agree with the adopted plan recommendations? If not, is the plan out-of-date or are there mitigating circumstances which speak to the nonconformity?



INDEX	
C0	DEMOLITION PLAN
C1	SITE PLAN
C1.1	ENLARGED SITE PLAN
C2	GRADING PLAN
C3	UTILITY PLAN
LS-1	LANDSCAPE PLANS ARCHITECTURAL PLANS

DEMOLITION KEY NOTES:

- (A) ALL UTILITIES SERVING STRUCTURES IMMEDIATELY SURROUNDING THE DEMOLITION BOUNDARY SHALL REMAIN IN SERVICE THROUGHOUT THE PROJECT. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PREVENT ANY DAMAGE TO SUCH UTILITIES. TYPICAL LOCATION.
- (B) THE CONTRACTOR SHALL REMOVE ALL UNDERGROUND/OVERHEAD ELECTRICAL SERVICES, TELEPHONE AND CABLE SERVICE LINES AND THEIR APPURTENANCES WITHIN THE PROJECT BOUNDARY. THE UTILITY SERVICES SHALL BE DISCONNECTED ALONG THE PROJECT BOUNDARY. TYPICAL LOCATION.
- (C) THE CONTRACTOR SHALL REMOVE EXISTING PRIVATE CURB.
- (D) REMOVE EXISTING PAVEMENT MARKING.
- (E) REMOVE EXISTING TRASH ENCLOSURE.
- (F) REMOVE EXISTING PUBLIC CONCRETE SIDEWALK SIDEWALK.
- (G) REMOVE EXISTING LIGHT POLE (RE: MEP SITE LIGHTING PLAN).
- (H) RELOCATE EXISTING STREET SIGN.

LEGEND

	PROPERTY LINE
	LOT LINE
	RIGHT-OF-WAY
	REMOVE EXISTING CURB & GUTTER
	EXISTING BUILDING TO BE REMOVED
	EXISTING ASPHALT PAVEMENT TO BE REMOVED
	EXISTING CONCRETE PAVEMENT/SIDEWALK TO BE REMOVED
	EXISTING GRAVEL TO BE REMOVED
	EXISTING TREE TO REMAIN
	REMOVE TREE
	EXISTING BURIED TELEPHONE
	EXISTING CABLE TELEVISION LINE
	EXISTING FIBER OPTIC LINE
	EXISTING WATER LINE
	EXISTING GAS LINE
	EXISTING BURIED ELECTRIC
	EXISTING OVERHEAD POWER LINE
	EXISTING SANITARY SEWER
	EXISTING STORM SEWER
	EXISTING FIRE HYDRANT
	EXISTING LIGHT POLE
	EXISTING CHAIN LINK FENCE

DEMOLITION NOTES:

- THE CONTRACTOR IS RESPONSIBLE FOR THE DEMOLITION, REMOVAL, AND DISPOSAL (IN A LOCATION APPROVED BY ALL GOVERNING AUTHORITIES) ALL CURBS, PARKING, DRIVES, DRAINAGE STRUCTURES, UTILITIES, ETC., SUCH THAT THE IMPROVEMENTS SHOWN ON THE REMAINING PLANS CAN BE CONSTRUCTED. ALL FACILITIES TO BE REMOVED SHALL BE UNDERCUT TO SUITABLE MATERIAL AND BROUGHT TO GRADE WITH SUITABLE COMPACTED FILL MATERIAL.
- THE CONTRACTOR IS RESPONSIBLE FOR REMOVING ALL DEBRIS FROM THE SITE AND DISPOSING THE DEBRIS IN A LAWFUL MANNER. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL PERMITS REQUIRED FOR DEMOLITION AND DISPOSAL.
- DAMAGE TO ALL EXISTING CONDITIONS TO REMAIN WILL BE REPLACED AT CONTRACTOR'S EXPENSE.
- CONTRACTOR MUST COORDINATE WITH OWNER PRIOR TO ANY CONSTRUCTION TO ESTABLISH CUSTOMER ACCESS AND TRAFFIC FLOW DURING ALL PHASES.
- REFER TO THE BUILDING PLANS FOR SITE LIGHTING ELECTRICAL MODIFICATIONS (IF ANY) TO THE EXISTING SYSTEM.

LEGAL DESCRIPTION:

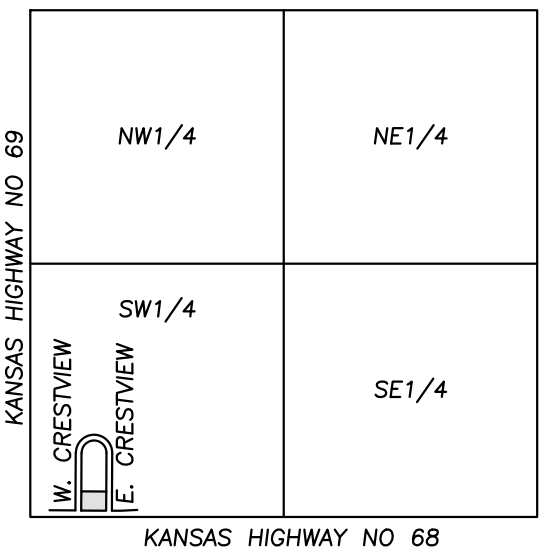
LOT 2 & 3, AMITY POINTE, a subdivision in the City of Louisburg, Miami County, Kansas.

GROSS AREA = ±1.509 ACRES / ±65,766 SQ.FT.

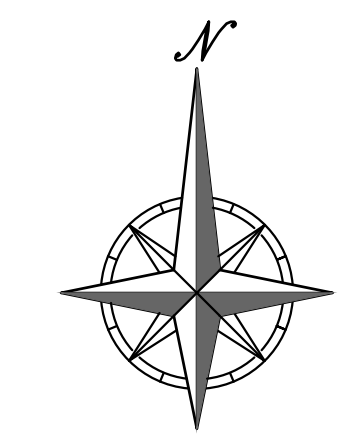


Know what's below.
Call before you dig.

UTILITY NOTES:
VISUAL INDICATIONS OF UTILITIES ARE AS SHOWN. UNDERGROUND LOCATIONS SHOWN, AS FURNISHED BY THEIR LESSORS, ARE APPROXIMATE AND SHOULD BE VERIFIED IN THE FIELD AT THE TIME OF CONSTRUCTION. FOR ACTUAL FIELD LOCATIONS OF UNDERGROUND UTILITIES CALL 811.



SCALE: 1"=2000'
VICINITY MAP
SEC. 30-16-25



SCALE: 1"=20'
0' 20' 40'

PHELPS ENGINEERING, INC.
1270 N. Winchester
Olathe, Kansas 66060
(913) 993-1155
Fax: (913) 993-1166
www.phelpsengineering.com

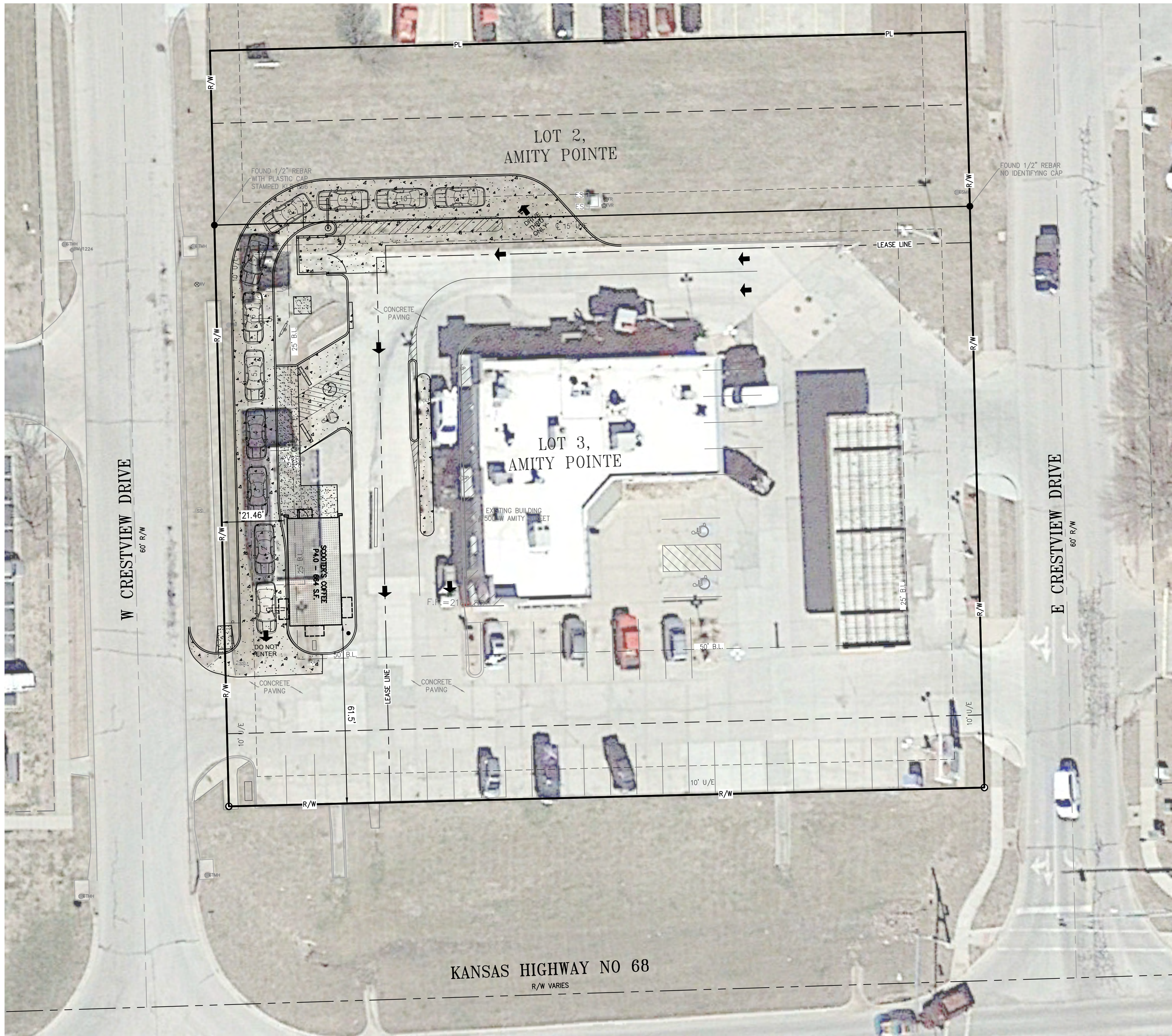
PLANNING
ENGINEERING
IMPLEMENTATION

DEMOLITION PLAN
SCOOTER'S COFFEE DRIVE - THRU
LOUISBURG, MIAMI COUNTY, KANSAS
ADDRESS: 1400 W AMITY STREET

PROJECT NO.	DATE	BY	APP.	REVISIONS
220625	07-05-2022	DAW/DOB		
		CHECKED: DAF	APPROVED: JDC	
		CITY OF LOUISBURG	DATE OF AUTHORIZATION	
		LAND SURVEYING - LS-82	LAND SURVEYING - E-38	
		CITY OF LOUISBURG	DATE OF AUTHORIZATION	
		LAND SURVEYING-200701028	LAND SURVEYING-200701028	
		ENGINEERING-200701028	ENGINEERING-200701028	

SHEET
C0

V:\PHELPS-SERVER\Projects\220625\Drawings\220625.dwg Plot:1 Jun 29, 2023 - 5:17pm Audrey Banks



SITE PLAN NOTES:

- All construction materials and procedures on this project shall conform to the latest revision of the following governing requirements, incorporated herein by reference:
 - City ordinances & O.S.H.A. Regulations.
 - The City of Olathe Technical Specifications and Municipal Code.
- The contractor shall have one (1) signed copy of the plans (approved by the City) and one (1) copy of the appropriate Design and Construction Standards and Specifications at the job site at all times.
- The contractor will be responsible for securing all permits, bonds and insurance required by the contract documents, City of Olathe, Kansas, and all other governing agencies (including local, county, state and federal authorities) having jurisdiction over the work proposed by these construction documents. The cost for all permits, bonds and insurance shall be the contractor's responsibility and shall be included in the bid for the work.
- The contractor is responsible for coordination of his and his sub-contractor's work. The contractor shall assume all responsibility for protecting and maintaining his work during the construction period and between the various trades/sub-contractors constructing the work.
- The demolition and removal (or relocation) of existing pavement, curbs, structures, utilities, and all other features necessary to construct the proposed improvements, shall be performed by the contractor. All waste material removed during construction shall be disposed off the project site. The contractor shall be responsible for all permits for hauling and disposing of waste material. The disposal of waste material shall be in accordance with all local, state and federal regulations.
- Contractor shall be responsible for all relocations, including but not limited to, all utilities, storm drainage, sanitary sewer services, signs, traffic signals & poles, etc. as required. All work shall be in accordance with governing authorities specifications and shall be approved by such. All cost shall be included in base bid.
- All existing utilities indicated on the drawings are according to the best information available to the Engineer; however, all utilities actually existing may not be shown. The contractor shall be responsible for contacting all utility companies for an exact field location of each utility prior to any construction. All underground utilities shall be protected at the contractor's expense. All utilities, shown and unshown, damaged through the negligence of the contractor shall be repaired or replaced by the contractor at his expense.
- The contractor will be responsible for all damage to existing utilities, pavement, fences, structures and other features not designated for removal. The contractor shall repair all damages at his expense.
- The contractor shall verify the flow lines of all existing storm or sanitary sewer connections and utility crossings prior to the start of construction. Notify the engineer of any discrepancies.
- SAFETY NOTICE TO CONTRACTOR:** In accordance with generally accepted construction practices, the contractor shall be solely and completely responsible for conditions of the job site, including safety of all persons and property during performance of the work. This requirement will apply continuously and not be limited to normal working hours. Any construction observation by the engineer of the contractor's performance is not intended to include review of the adequacy of the contractor's safety measures, in, on or near the construction site.
- Refer to the building plans for site lighting electrical requirements, including conduits, pole bases, pull boxes, etc.

SITE DIMENSION NOTES:

- BUILDING TIES SHOWN ARE TO THE OUTSIDE FACE OF PROPOSED WALLS. THE SUBCONTRACTOR SHALL REFER TO THE ARCHITECTURAL PLANS FOR SPECIFIC DIMENSIONS AND LAYOUT INFORMATION FOR THE BUILDINGS.
- ALL DIMENSIONS SHOWN FOR THE PARKING LOT AND CURBS ARE MEASURED FORM BACK OF CURB TO BACK OF CURB.

PAVEMENT MARKING AND SIGNAGE NOTES:

- PARKING STALL MARKING STRIPES SHALL BE FOUR INCH (4") WIDE WHITE STRIPES. DIRECTIONAL ARROW AND HANDICAP STALL MARKINGS SHALL BE FURNISHED AT LOCATIONS SHOWN ON PLANS.
- HANDICAP PAVEMENT MARKINGS AND SIGNS SHALL CONFORM TO ALL FEDERAL (AMERICANS WITH DISABILITIES ACT) AND STATE LAWS AND REGULATIONS.
- TRAFFIC CONTROL DEVICES AND PAVEMENT MARKINGS SHALL CONFORM TO THE REQUIREMENTS OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES".
- STOP SIGNS SHALL BE PROVIDED AT ALL LOCATIONS AS SHOWN ON PLANS AND SHALL CONFORM TO THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES". SIGNS SHALL BE 18" X 12", 18 GAUGE STEEL AND SHALL BE ENGINEER GRADE REFLECTIVE.
- TRAFFIC CONTROL AND PAVEMENT MARKINGS SHALL BE PAINTED WITH A WHITE SHERWIN WILLIAMS S-W TRAFFIC MARKING SERIES B-2912 OR APPROVED EQUAL. THE PAVEMENT MARKING SHALL BE APPLIED IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS. APPLY ON A CLEAN, DRY SURFACE AND AT A SURFACE TEMPERATURE OF NOT LESS THAN 70°F AND THE AMBIENT AIR TEMPERATURE SHALL NOT BE LESS THAN 60°F AND RISING. TWO COATS SHALL BE APPLIED.

LEGAL DESCRIPTION:

LOT 2 & 3, AMITY POINTE, a subdivision in the City of Louisburg, Miami County, Kansas.

GROSS AREA = ±1.509 ACRES / ±65,766 SQ.FT.

FLOOD NOTE:

THIS PROPERTY LIES WITHIN ZONE X, DEFINED AS AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN, AS SHOWN ON THE FLOOD INSURANCE RATE MAP PREPARED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY FOR THE CITY OF LOUISBURG, COMMUNITY NO. 200222, MIAMI COUNTY, KANSAS, MAP NO. 20121C02050, AND DATED JANUARY 16, 2014.



UTILITY NOTES:
VISUAL INDICATIONS OF UTILITIES ARE SHOWN. UNDERGROUND LOCATIONS SHOWN, AS FURNISHED BY THEIR LESSORS, ARE APPROXIMATE AND SHOULD BE VERIFIED IN THE FIELD AT THE TIME OF CONSTRUCTION. FOR ACTUAL FIELD LOCATIONS OF UNDERGROUND UTILITIES CALL 811.

SITE DATA

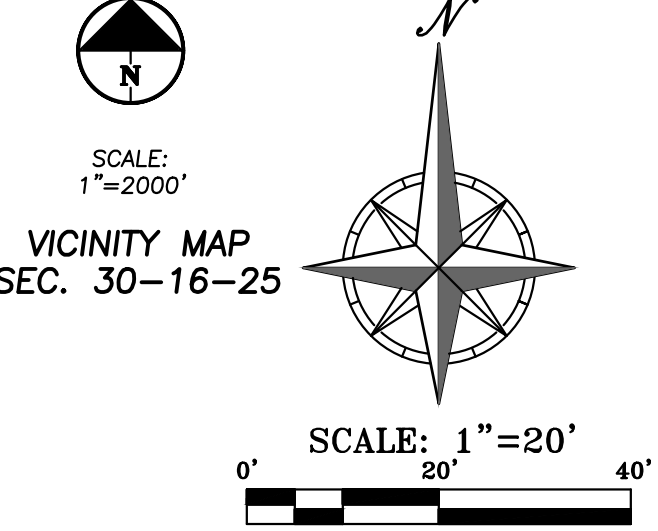
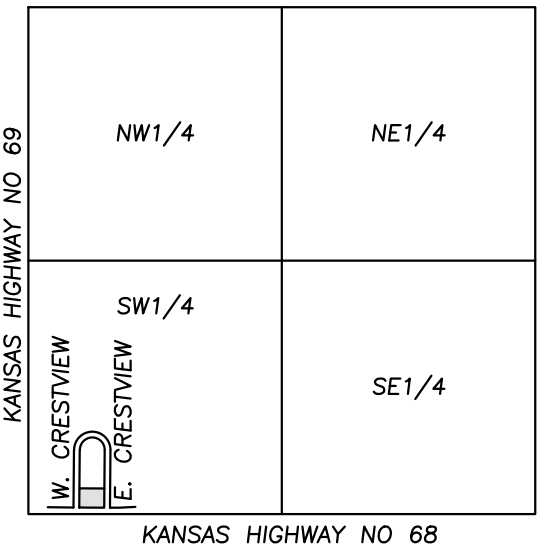
LOT 2 & 3	65,766 S.F. / 1.509 AC.
EXISTING BUILDING S.F.	
MINI MART/CONVENIENCE STORE	1,974 S.F.
FAST FOOD	2,955 S.F.
PROPOSED BUILDING	
DRIVE THRU COFFEE SHOP	664 S.F.

PARKING SUMMARY

EXISTING PARKING	
STANDARD STALLS (9'-0" X 18'-0")	47
ACCESSIBLE STALLS	2
TOTAL STALLS	49
PROPOSED PARKING	
STANDARD STALLS (9'-0" X 18'-0")	37
ACCESSIBLE STALLS	3
TOTAL STALLS	41

LEGEND

- PL — PROPERTY LINE
- LL --- LEASE LINE
- - - R/W - - - RIGHT-OF-WAY
- 2' CURB & GUTTER
- 6" CURB
- B/L — BUILDING SETBACK LINE
- P/S — PARKING SETBACK LINE
- L/S — LANDSCAPE SETBACK LINE
- [Pattern] PROPOSED BUILDING
- [Pattern] CONCRETE PAVEMENT
- [Pattern] CONCRETE SIDEWALK



PHELPS ENGINEERING, INC.
1370 N. Winchester
Olathe, Kansas 66061
(913) 993-1155
Fax (913) 993-1165
www.phelpsengineering.com

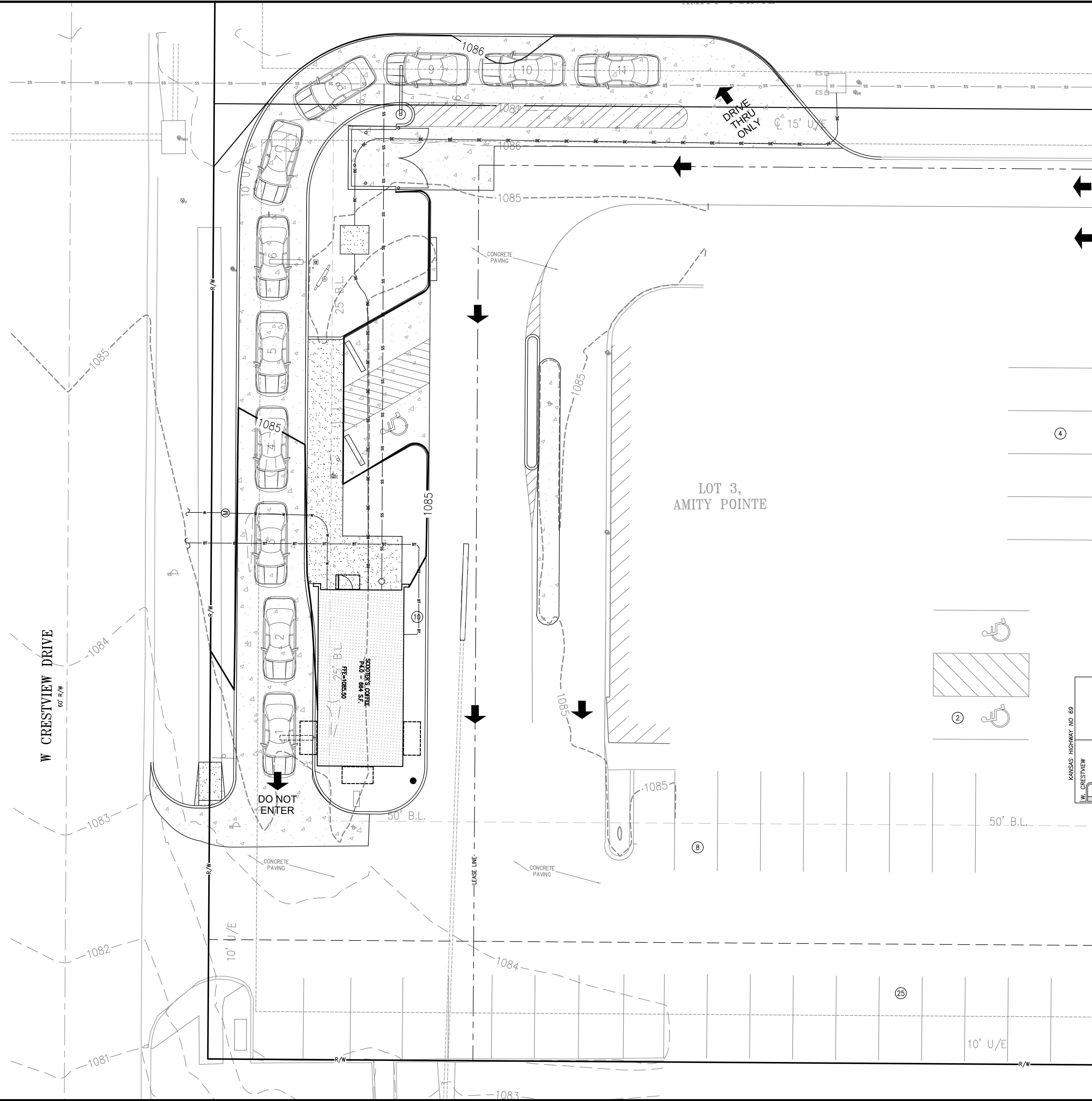
PLANNING
ENGINEERING
IMPLEMENTATION

OVERALL SITE PLAN
SCOOTER'S COFFEE DRIVE - THRU
LOUISBURG, MIAMI COUNTY, KANSAS
ADDRESS: 1400 W AMITY STREET

Project No.	220625	Date	By	App.
DATE	07-05-2022			
CHECKER	DAF			
DATE OF AUTHORIZATION				
LAND SURVEYING - LS-82				
ENGINEERING - E-361				
DATE OF AUTHORIZATION				
LAND SURVEYING-20070128				
ENGINEERING-20030308				

SHEET
C1

\PHILIPS-SERVER\Projects\220625\Drawings\220625.dwg Layout1 Jun 28, 2023 - 5:17pm Audrey Burns



SITE GRADING NOTES:

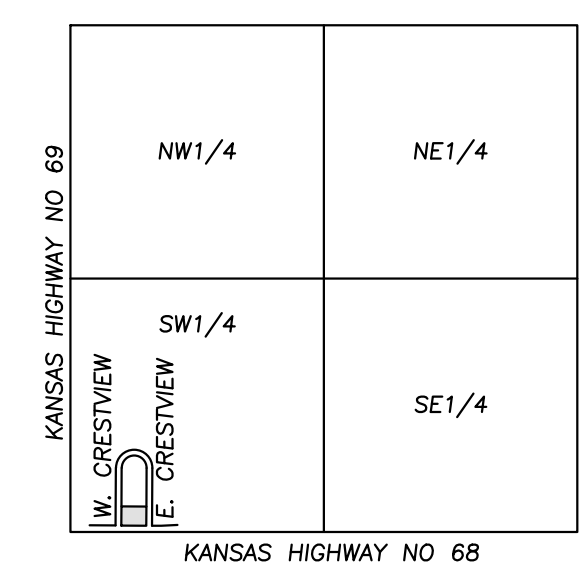
- CONTOURS AND ELEVATIONS: Existing and proposed contours are shown on plans at one foot (1') contour intervals, unless otherwise noted. Proposed contours and elevations shown represent approximate finish grade. Contractor shall hold down subgrades to allow for pavement and sub-base thicknesses.
- If the contractor does not accept existing topography as shown on the plans, without exception, he shall have made at his expense, a topographic survey by a registered land surveyor and submit it to the owner for review.
- CLEARING AND GRUBBING: Prior to beginning preparation of subgrade, all areas under pavements or building shall be stripped of all topsoil, vegetation, large rock fragments (greater than 6 inches in any dimension) and any other deleterious material. Actual stripping depth should be based on visual examination during construction and the results of proof-rolling operations. The root systems of all trees (not designated to remain) shall be removed in their entirety. Stripping materials shall not be incorporated into structural fills.
- TOPSOIL STRIPPING: Prior to the start of site grading, the contractor shall strip all topsoil from areas to be graded, and stockpiled at a location on or adjacent to the site as directed by the owner. At completion of grading operations and related construction, the contractor will be responsible for redistribution of topsoil over all areas disturbed by the construction activities. Topsoil shall be placed to a minimum depth of six inches (6") and in accordance with specifications for landscaping. At that time, and prior to the installation of landscaping or irrigation, all topsoil graded areas shall be visually inspected and accepted by the owner and IFL.
- Contractor shall adjust and/or cut existing pavement as necessary to assure a smooth fit and continuous grade. Contractor shall assure positive drainage away from buildings for all natural and paved areas.
- SUBGRADE PREPARATION: Prior to placement of new fill material, the existing subgrade shall be proofrolled and approved under the direction of the Geotechnical Engineer or his representative.
- PROOFROLLING: Subsequent to completion of stripping and over-excavation, all building and pavement areas to receive engineered fill should be systematically proof-rolled using a tandem axle dump truck loaded to approximately 20,000 pounds per axle. Also, any finished subgrade areas to receive paving shall be proof-rolled within 48 hours of paving. Unsuitable soils that are detected and that can not be recompacted should be over-excavated and replaced with controlled structural fill.
- EARTHWORK:
 - GEOTECHNICAL: All earthwork shall conform to the recommendations of the Geotechnical report. Said report and its recommendations are herein incorporated into the project requirements by reference. Prior to beginning construction, the contractor shall obtain a copy of and become familiar with the geotechnical report. Unless specifically noted on the plans, the recommendations in the geotechnical report are hereby incorporated into the project requirements and specifications.
 - SURFACE WATER: Surface water shall be intercepted and diverted during the placement of fill.
 - FILLS: All fills shall be considered controlled or structural fill and shall be free of vegetation, organic matter, topsoil and debris. In areas where the thickness of the engineered fill is greater than five feet building and pavement construction should not commence until so authorized by the on-site geotechnical engineer to allow for consolidation.
 - BUILDING SUBGRADE: As specified in the Geotechnical Engineering Report, the upper section of building subgrade shall consist of Low Volume Change (LVC) material defined as approved, compacted granular fill or low to moderate plasticity cohesive soil materials stabilized with Class C Flyash. Granular fill shall consist of compacted granular materials with a maximum particle size of two (2) inches or less, such as limestone screenings. Refer to geotechnical report for complete requirements.
 - EXISTING SLOPES: Where fill material is to be placed on existing slopes greater than 5:1 (horizontal to vertical), existing slope shall be benched providing a minimum vertical face of twelve inches (12"). The benches should be cut wide enough to accommodate the compaction equipment. Fill material shall be placed and compacted in horizontal lifts not exceeding nine inches (9") (loose lift measurement), unless otherwise approved by the Geotechnical Engineer.
 - COMPACTION REQUIREMENTS: The upper 9 inches of pavement subgrade areas shall be compacted to a minimum density of ninety five percent (95%) of the material's maximum dry density as determined by ASTM D698 (standard proctor compaction). The moisture content at the time of placement and compaction shall be within a range of 2% below to 4% above optimum moisture content as defined by the standard proctor compaction procedure. The moisture contents shall be maintained within this range until completion of the work. Where compaction of earth fill by a large roller is impractical or undesirable, the earth fill shall be hand compacted with small vibrating rollers or mechanical tampers.
- All cut or fill slopes shall be 3:1 or flatter. All asphalt parking areas shall be a minimum of 1% slope but not more than 5% slope unless otherwise noted. All pavements within ADA parking areas shall not exceed 2% total slope. All grades around building shall be held down 6" from finish floor and slope away another 6" in 10 feet. Contractor shall notify engineer prior to final subgrade construction of any areas not within this slope requirement.
- TESTING AND INSPECTION: Owner's Independent Testing Laboratory (ITL) shall make tests of earthwork during construction and observe the placement of fills and other work performed on this project to verify that work has been completed in accordance with Geotechnical Engineering Report, Project Specifications and within industry standards. The ITL will be selected by the owner and the cost of testing will be the owner's responsibility.
- CLASSIFICATION: All excavation shall be considered unclassified. No separate or additional payments shall be made for rock excavation.
- PERMANENT RESTORATION: All areas disturbed by earthwork operations shall be sodded, unless shown otherwise by the landscaping plan or erosion control plan.
- UTILITIES: The contractor is specifically cautioned that the location and/or elevation of existing utilities as shown on these plans is based on records of the various utility companies, and where possible, measurements taken in the field. The information is not to be relied on as being exact or complete. The contractor must call the appropriate utility companies at least 48 hours before any excavation to request exact field location of utilities. It shall be the responsibility of the contractor to relocate all existing utilities which conflict with the proposed improvements shown on the plans.
- LAND DISTURBANCE: The contractor shall adhere to all terms & conditions as outlined in the EPA or applicable state N.P.D.E.S. permit for storm water discharge associated with construction activities. Refer to project S.W.P.P.P. requirements.

FLOOD NOTE:

THIS PROPERTY LIES WITHIN ZONE X, DEFINED AS AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN, AS SHOWN ON THE FLOOD INSURANCE RATE MAP PREPARED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY FOR THE CITY OF LOUISBURG, COMMUNITY NO. 200222, MIAMI COUNTY, KANSAS, MAP NO. 20121002050, AND DATED JANUARY 16, 2014.

LEGEND

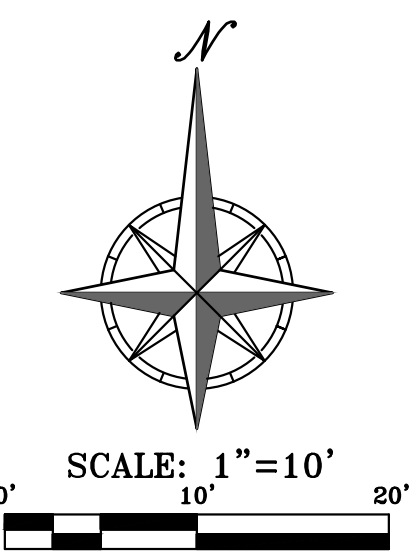
- PL — PROPERTY LINE
- LL — LOT LINE
- R/W — RIGHT-OF-WAY
- 2' CURB & GUTTER
- 918 — EXISTING CONTOURS
- 920 — PROPOSED CONTOURS
- 918 — PROPOSED CONTOURS
- PROPOSED SPOT ELEVATION
- LG LIP OF GUTTER
- TC TOP OF CURB
- SW SIDEWALK
- ME MATCH EXISTING
- HP HIGH POINT
- LP LOW POINT
- P TOP OF PAVEMENT
- TE TOP OF STRUCTURE
- GR GROUND ELEVATION
- BS BOTTOM OF STEPS
- TS TOP OF STEPS
- BW BOTTOM OF WALL
- TW TOP OF WALL
- — EXISTING STORM SEWER



SCALE: 1"=2000'
VICINITY MAP
SEC. 30-16-25



UTILITY NOTES:
VISUAL INDICATIONS OF UTILITIES ARE AS SHOWN. UNDERGROUND LOCATIONS SHOWN, AS FURNISHED BY THEIR LESSORS, ARE APPROXIMATE AND SHOULD BE VERIFIED IN THE FIELD AT THE TIME OF CONSTRUCTION. FOR ACTUAL FIELD LOCATIONS OF UNDERGROUND UTILITIES CALL 811.



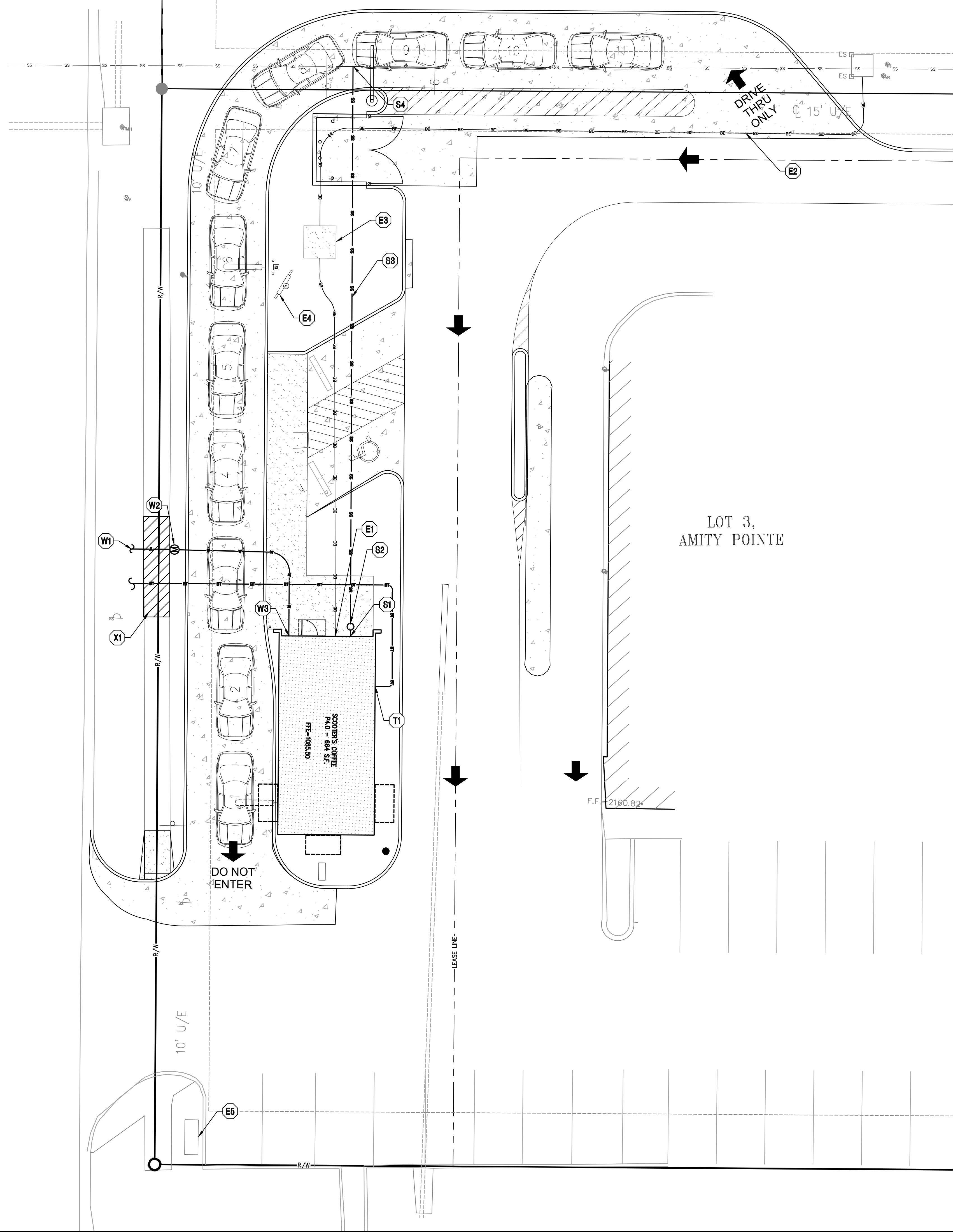
PHELPS ENGINEERING, INC.
 1270 N. Winchester
 Olathe, Kansas 66061
 (913) 993-1155
 Fax: (913) 993-1165
 www.phelpsengineering.com

GRADING PLAN
 SCOOTER'S COFFEE DRIVE - THRU
 LOUISBURG, MIAMI COUNTY, KANSAS
 ADDRESS: 1400 W AMITY STREET

No.	Date	Revisions:

PROJECT NO. 220625
 DATE: 07-05-2023
 CHECKER: DAF APPROVED: JJC
 CADD: JJC
 LAND SURVEYING - LS-82
 ENGINEERING - E-36
 STATE OF AUTHORIZATION
 LAND SURVEYING: 200700128
 ENGINEERING: 200700308

SHEET
C2



UTILITY KEY NOTES:

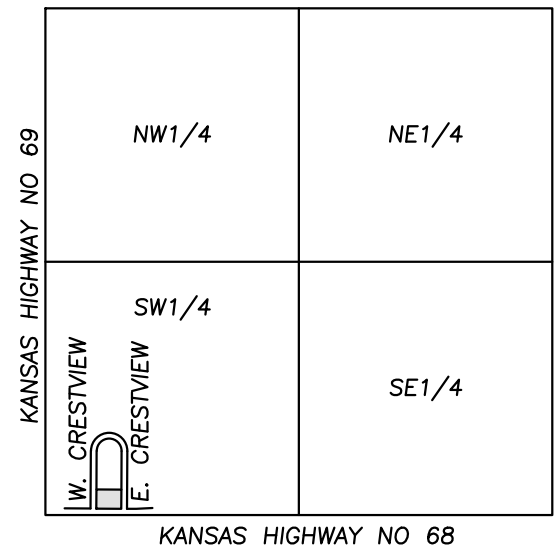
- (E1) ELECTRIC ENTRY INTO BUILDING. FOLLOW ELECTRIC COMPANY ORDER AND SPECIFICATIONS FOR PRIMARY ELECTRICAL SERVICE AND ROUTING TO POWER SOURCE.
- (E2) INSTALL PRIMARY ELECTRICAL CONDUIT. FOLLOW ELECTRIC COMPANY WORK ORDER AND SPECIFICATIONS FOR PRIMARY ELECTRICAL SERVICE AND ROUTING TO POWER SOURCE.
- (E3) INSTALL CONCRETE TRANSFORMER PAD. CONTRACTOR TO VERIFY EXACT LOCATION AND SIZE WITH ELECTRIC COMPANY PRIOR TO CONSTRUCTION. CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLATION OF CONCRETE PAD AND CONDUIT AS REQUIRED BY THE ELECTRIC COMPANY. CONTRACTOR SHALL COORDINATE SAID WORK WITH THE ELECTRIC COMPANY.
- (E4) CONTRACTOR TO COORDINATE ROUTING OF CONDUITS FOR POWER TO ORDER MENU BOARD (RE: SITE LIGHTING PLAN).
- (E6) CONTRACTOR TO COORDINATE ROUTING OF CONDUITS FOR POWER TO MONUMENT SIGN (RE: SITE LIGHTING PLAN).
- (T1) CONTRACTOR IS RESPONSIBLE TO COORDINATE WITH TELEPHONE COMPANY FOR THE INSTALLATION OF BURIED TELEPHONE LINES. CONTRACTOR TO PROVIDE TWO (2)-4" PVC SCH. 40 CONDUITS WITH PULL STRINGS FROM BUILDING TO FEED POINT. CONTRACTOR TO VERIFY EXACT ROUTING AND FEED POINT WITH TELEPHONE COMPANY.
- (S1) CONNECT TO INTERIOR PLUMBING. MAINTAIN A 4" PIPE FROM INTERIOR TO EXTERIOR AT FOUNDATION WALL. FG=1085.50 FL=1081.50
- (S2) INSTALL CLEANOUT (TYP).
- (S3) INSTALL 4" PVC (SDR-26) AT 2.24% SLOPE.
- (S4) CONNECT TO EXISTING 6" SANITARY SEWER. EX. 6" FL = 1078.49 PROP. 4" FL = 1079.18
- (W1) CONTRACTOR TO LOCATE EXISTING WATER LINE AND COORDINATE 1" TAP FOR PROPOSED BUILDING DOMESTIC SERVICE. CONTACT UTILITY PROVIDER FOR TAPPING REQUIREMENTS. CONTRACTOR TO PAY ALL FEES FOR WATER MAIN TAP. OWNER WILL REIMBURSE CONTRACTOR FOR METER OR SYSTEM DEVELOPMENT FEES.
- (W2) PROVIDE AND INSTALL 1" PRIMARY WATER METER PIT PER CITY REQUIREMENTS. CONTRACTOR TO COORDINATE AND PAY ALL FEES. ALL LABOR AND MATERIALS SHALL BE PROVIDED AND INSTALLED BY THE CONTRACTOR'S PLUMBER IN ACCORDANCE WITH CITY STANDARDS. TRANSITION FROM 1" TO 1-1/4" DOMESTIC WATER LINE 5 FEET DOWNSTREAM OF WATER METER.
- (W3) 1 1/4" DOMESTIC WATERLINE ENTRY TO BUILDING. CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING ANY APPURTENANCES ON THE DOMESTIC LINE SUCH AS BACKFLOW PREVENTION DEVICES (RE: BUILDING PLANS), GATE VALVES, REDUCERS, BENDS, TEES, ETC., WHICH MAY BE REQUIRED. CONTRACTOR TO COORDINATE WITH WATER UTILITY.
- (X1) CONTRACTOR TO REMOVE & REPLACE EXISTING PUBLIC SIDEWALK, AS NEEDED, FOR UTILITY INSTALLATION.

UTILITY NOTES:

1. The contractor is specifically cautioned that the location and/or elevation of existing utilities as shown on these plans is based on records of the various utility companies, and where possible, measurements taken in the field. The information is not to be relied on as being exact or complete. The contractor must call the appropriate utility companies at least 48 hours before any excavation to request exact field location of utilities. It shall be the responsibility of the contractor to coordinate with and relocate &/or remove all existing utilities which conflict with the proposed improvements shown on the plans.
2. The construction of storm sewers on this project shall conform to the requirements of the City's Technical Specifications and Design Criteria.
3. The contractor shall field verify the exact location and elevation of the existing storm sewer lines and the existing elevation at locations where the proposed storm sewer is to be installed. If discrepancies are encountered from the information shown on the plans, the contractor shall contact the design engineer. No pipes shall be laid until direction is received from the design engineer.
4. It will be the contractor's responsibility to field adjust the top of all manholes and boxes as necessary to match the grade of the adjacent area. Tops of existing manholes shall be raised as necessary with proposed pavement elevations, and to be 6-inches above finished ground elevations in non-paved areas. No separate or additional compensation will be made to the contractor for making final adjustments to the manholes and boxes.
5. Inlet locations, horizontal pipe information and vertical pipe information is shown to the center of the structure. Deflection angles shown for storm sewer pipes are measured from the center of curb inlets and manholes. The contractor shall adjust the horizontal location of the pipes to go to the face of the boxes. All roof drains shall be connected to storm sewer structures. Provide cleanouts on roof drain lines at 100' max. Spacing and at all bend points. Do not connect roof drains directly to storm sewer pipe.
6. The contractor shall be responsible for furnishing and installing all fire and domestic water lines, meters, backflow devices, pits, valves and all other incidentals required for a complete operable fire protection and domestic water system. All costs associated with the complete water system for the buildings shall be the responsibility of the contractor. All work shall conform to the requirements of City.
7. The contractor shall be responsible for furnishing and installing all sanitary sewer service lines from the buildings to the public line. All work shall conform to the requirements of the City and Johnson County Unified Wastewater District.
8. The contractor will be responsible for securing all permits, bonds and insurance required by the contract documents, City, and all other governing agencies (including local, county, state and federal authorities) having jurisdiction over the work proposed by these construction documents. The cost for all permits bonds and insurance shall be the contractor's responsibility and shall be included in the bid for the work.
9. By the use of these construction documents the contractor hereby agrees that he/she shall be solely responsible for the safety of the construction workers and the public. The contractor agrees to hold the engineer and owner harmless for any and all injuries, claims, losses or damages related to the project.
10. The contractor shall be responsible for furnishing all materials, tools and equipment and installation of electrical power, telephone and gas service from a point of connection to the public utility lines to the building structures. This will include all conduits, service lines, meters, concrete pads and all other incidentals required for a complete and operational system as required by the owner and the public utilities. Refer to building plans for exact tie-in locations of all utilities. Contractor shall verify connection points prior to installation of utility line.
11. All fill material is to be in place, compacted, and consolidated before installation of proposed utilities. On-site geotechnical engineer shall provide written confirmation that this requirement has been met and that utilities may proceed in the fill areas. All utilities are to be placed in trench conditions.
12. Contractor shall notify the utility authorities inspectors 48 hours before connecting to any existing line.
13. Water lines shall be as follows (unless otherwise shown on plans):
 - A. Pipe sizes less than 3-inches that are installed below grade and outside building shall comply with the following:
 1. Seamless Copper Tubing: Type "K" soft copper, ASTM B88.
 2. Fittings: Wrought copper (#5.5 Tin Antimony solder joint), ASME B 16.22.
 - B. Pipe sizes 3-inches through 48-inches that are installed below grade and outside building shall comply with one of the following:
 1. Gray Cast Iron Water Pipe: ANSI A21.6, thickness class 52.
 - a. Fittings: Either mechanical joint or push-on joint, AWWA C110 or AWWA C111.
 - b. Elastomeric gaskets and lubricant: ASTM F477.
 - c. Cement Mortar Lining, AWWA C104
 - 2. Ductile Iron Water Pipe: AWWA C151, thickness class 50.
 - a. Fittings: Either mechanical joint or push-on joint, AWWA C110 or AWWA C111.
 - b. Elastomeric gaskets and lubricant: ASTM F477.
 - c. Cement Mortar Lining, AWWA C104
 - 3. Polyvinyl Chloride (PVC) Water Pipe: Pipe, AWWA C900, rated DR 18 (Class 150), continually marked as required.
 - a. Elastomeric gaskets and lubricant: ASTM F477 for smaller pipes.
 - b. Pipe joints: Integrally molded bell ends, ASTM D3139.
 - c. Trace wire: Magnetic detectable conductor, (#12 Copper) brightly colored plastic covering imprinted with "Water Service" in large letters
14. Minimum trench width shall be 2 feet.
15. Contractor shall maintain a minimum of 42" cover on all waterlines. All water line joints are to be mechanical joints with thrust blocking as called out in specifications and construction plans. Water mains and service lines shall be constructed in accordance to waterline's specifications for commercial services.
16. All waterlines shall be kept min. ten (10') apart (parallel) from sanitary sewer lines or manholes. Or when crossing, an 24" vertical clearance (outside edge of pipe to outside edge of pipe) of the water line above the sewer line is required.
17. Sanitary conflicts will be resolved prior to permit issuance.
18. In the event of a vertical conflict between waterlines, sanitary lines, storm lines and gas lines (existing and proposed), the sanitary line shall be ductile iron pipe with mechanical joints at least 10 feet on both sides of crossing (or encased in concrete this same distance), the waterline shall have mechanical joints with appropriate thrust blocking as required to provide a minimum of 24" clearance. Meeting requirements of ANSI A21.10 or ANSI 21.11 (AWWA C-151) (CLASS 50).
19. All underground storm, sanitary, water and other utility lines shall be installed, inspected and approved before backfilling. Failure to have inspection approval prior to backfill will constitute rejection of work.
20. All necessary inspections and/or certifications required by codes and/or utility service companies shall be performed prior to announced building possession and the final connection of service. Contractor shall coordinate with all utility companies for installation requirements and specifications.
21. Refer to building plans for site lighting electrical plan, irrigation, parking lot security system and associated conduit requirements. Coordinate with Owner that all required conduits are in place & tested prior to paving.
22. When a building utility connection from site utilities leading up to the building cannot be made immediately, temporarily mark all such site utility terminations.
23. Refer to the building plans for site lighting electrical requirements, including conduits, pole bases, pull boxes, etc.

LEGEND

- PL PROPERTY LINE
- LL LOT LINE
- R/W RIGHT-OF-WAY
- CATV EXISTING CABLE TELEVISION LINE
- FO EXISTING FIBER OPTIC LINE
- G EXISTING GAS LINE
- BE EXISTING BURIED ELECTRIC LINE
- OHP EXISTING OVERHEAD POWER LINE
- OHT EXISTING OVERHEAD TELEPHONE LINE
- SS EXISTING SANITARY SEWER LINE
- SS 24"HDPE EXISTING STORM SEWER LINE (& SIZE)
- BT EXISTING BURIED TELEPHONE LINE
- W-6" EXISTING WATER LINE (& SIZE)
- F-6" EXISTING FIRE LINE (& SIZE)
- ST-6" EXISTING ROOF DRAIN (& SIZE)
- CATV PROPOSED CABLE TELEVISION LINE
- FO PROPOSED FIBER OPTIC LINE
- G PROPOSED GAS LINE
- BE PROPOSED BURIED ELECTRIC LINE
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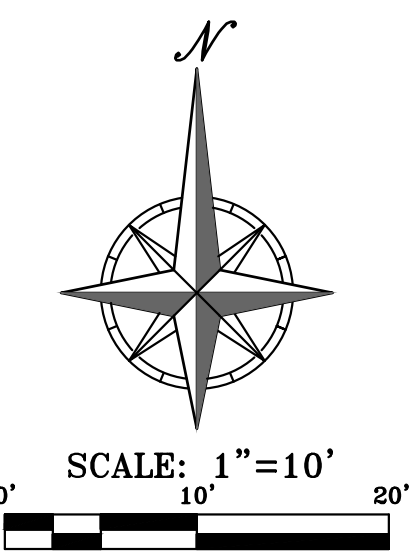


SCALE: 1"=2000'
VICINITY MAP
SEC. 30-16-25



Know what's below.
Call before you dig.

UTILITY NOTES:
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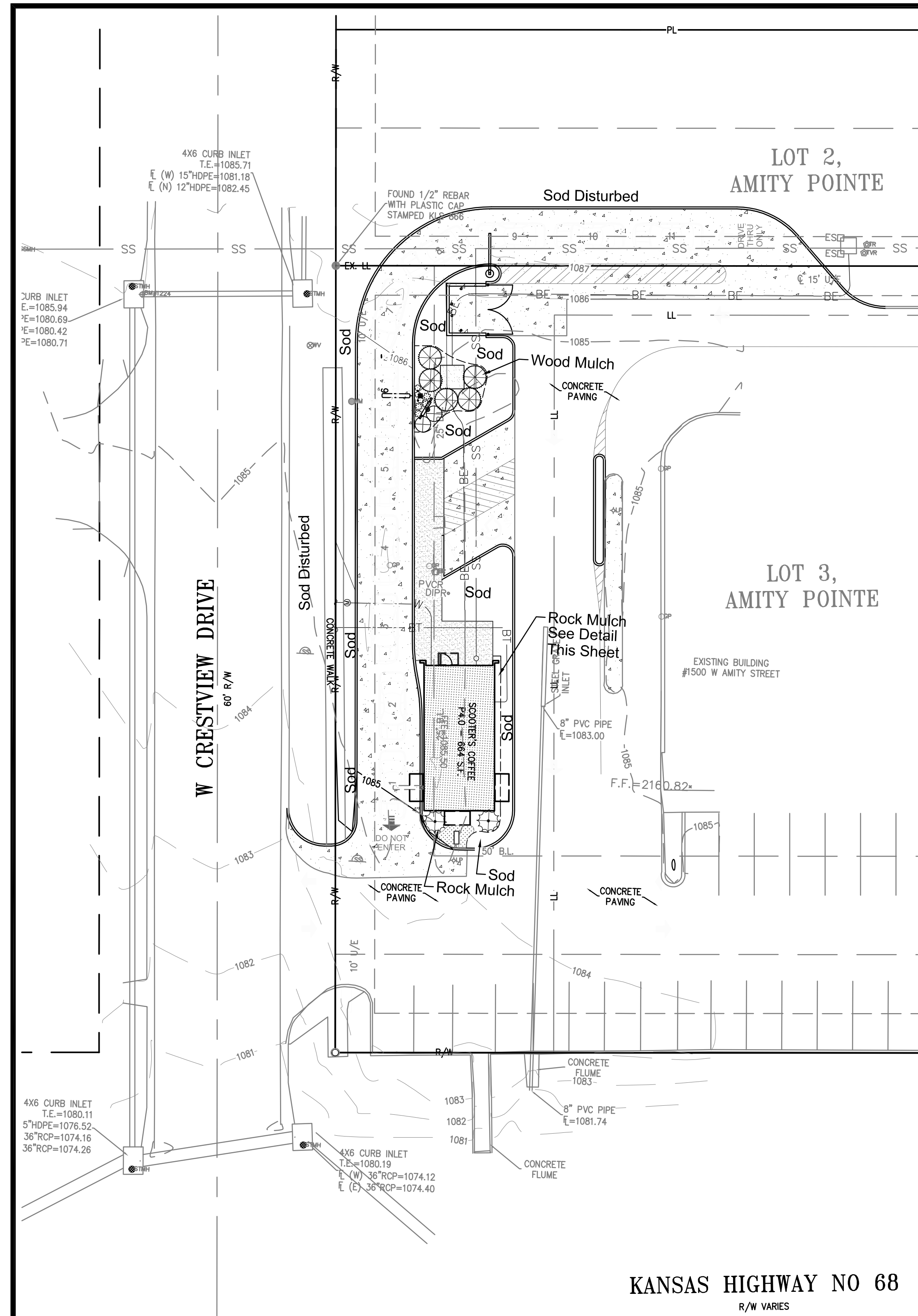
PHILIPS ENGINEERING, INC.
1270 N. Winchester
Olathe, Kansas 66066
(913) 993-1155
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www.philipsengineering.com

PLANNING
ENGINEERING
IMPLEMENTATION

UTILITY PLAN
SCOOTER'S COFFEE DRIVE - THRU
LOUISBURG, MIAMI COUNTY, KANSAS
ADDRESS: 1400 W AMITY STREET

PROJECT NO.	DATE	BY	APP.
220625	07-05-2022	DAVID DDB	
		CHECKED: DAF APPROVED: JJC	
		CORPORATE SEAL OF AUTHORIZATION	
		LAND SURVEYING - LS-82	
		ENGINEERING - E-36	
		CERTIFICATE OF AUTHORIZATION	
		LAND SURVEYING-200701028	
		ENGINEERING-200300308	

SHEET
C3

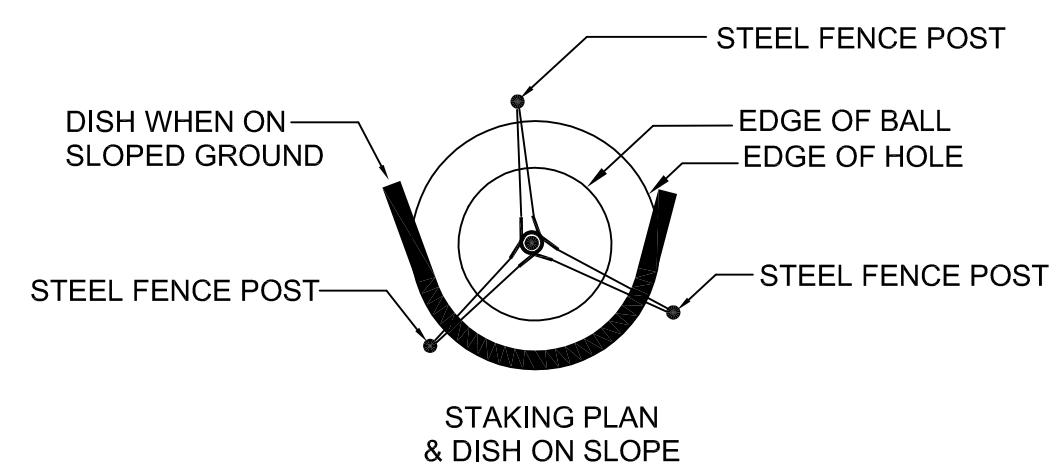


PLANT_SCHEDULE

SHRUBS	QTY	BOTANICAL / COMMON NAME	CONT
	5	Juniperus chinensis 'Sea Green' / Sea Green Juniper 24"-30" hgt. & sp.	5 gal
	2	Juniperus virginiana 'Grey Owl' / Grey Owl Juniper 24" sp.	3 gal
	4	Rosa x 'Novarosop' / Popcorn Drift Rose 12"-15" sp.	1 gal
	3	Sedum spectabile 'Autumn Fire' / Showy Stonecrop 15"-18" hgt. & sp.	1 gal
	2	Spiraea x bumalda 'Gold Flame' / Gold Flame Spiraea 18"-24" hgt.	3 gal
	37 sf.	Seasonal Annuals	4" pots

GENERAL LANDSCAPE NOTES:

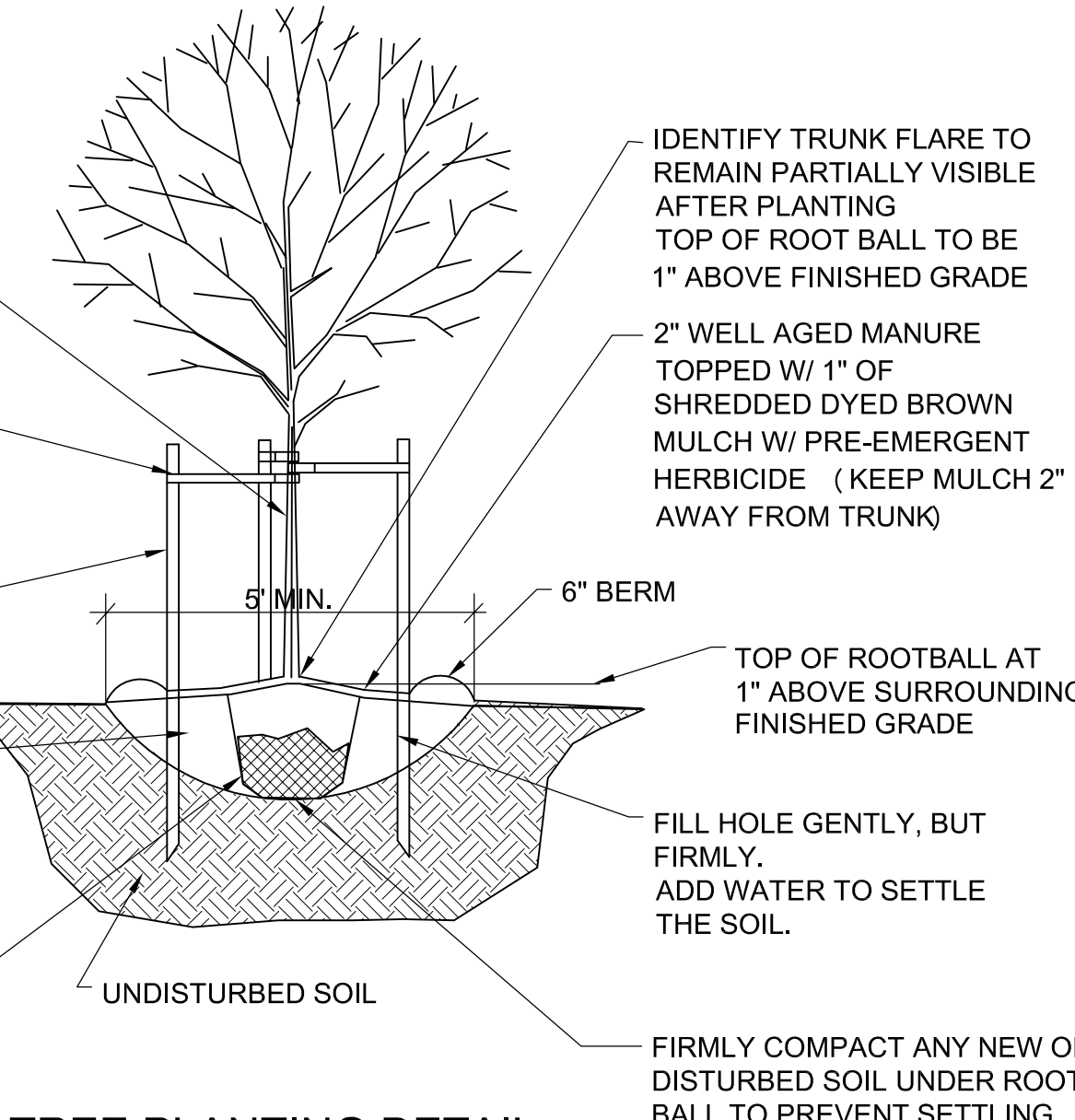
- CONTRACTOR SHALL VERIFY THE EXISTENCE AND LOCATION OF ALL UTILITIES BEFORE STARTING ANY WORK.
- CONTRACTOR SHALL VERIFY ALL LANDSCAPE MATERIAL QUANTITIES AND SHALL REPORT ANY DISCREPANCIES TO THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
- CONTRACTOR SHALL MAKE NO SUBSTITUTIONS WITHOUT THE APPROVAL OF THE LANDSCAPE ARCHITECT.
- CONTRACTOR SHALL STAKE LAYOUT PLAN IN THE FIELD AND SHALL HAVE THE LAYOUT APPROVED BY THE LANDSCAPE ARCHITECT BEFORE PROCEEDING WITH THE INSTALLATION.
- ALL LANDSCAPE BEDS SHALL BE TREATED WITH THE PRE-EMERGENT HERBICIDE PRE-M 60 DG (GRANULAR) OR AN APPROVED EQUAL IN ACCORDANCE WITH THE MANUFACTURER'S INSTRUCTIONS.
- ALL LANDSCAPE BEDS SHALL RECEIVE A TRENCHED EDGE. SEE SHRUB PLANTING DETAIL. FINISH GRADE OF BEDS SHALL FLOW WITH SLOPE, NOT BE MOUNDED, AND BE AT ADJACENT PAVED SURFACE LEVEL.
- FERTILIZER FOR FESCUE SODDED AREAS, TREES AND CONTAINER STOCK AREAS SHALL BE A BALANCED FERTILIZER BASED ON RECOMMENDATIONS FROM A SOIL TEST SUPPLIED BY THE LANDSCAPE CONTRACTOR FROM AN APPROVED TESTING LAB.
- CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE OF THE PLANTS UNTIL COMPLETION OF THE JOB AND ACCEPTANCE BY THE OWNER.
- CONTRACTOR SHALL WARRANTY ALL LANDSCAPE WORK AND PLANT MATERIAL FOR A PERIOD OF ONE YEAR FROM DATE OF ACCEPTANCE OF THE WORK BY THE OWNER.
- CONTRACTOR SHALL PROVIDE MAINTENANCE OF ALL TREES AND SHRUBS FOR A PERIOD OF ONE YEAR AFTER THE DATE OF SUBSTANTIAL COMPLETION IF CONTRACTED BY THE OWNER.
- ANY PLANT MATERIAL WHICH DIES DURING THE ONE YEAR WARRANTY PERIOD SHALL BE REPLACED BY THE CONTRACTOR DURING NORMAL PLANTING SEASONS.
- ALL PLANT NAMES ON THE PLANT LIST CONFORM TO THE STANDARDIZED PLANT NAMES PREPARED BY THE AMERICAN JOINT COMMITTEE ON HORTICULTURAL NOMENCLATURE OR TO NAMES GENERALLY ACCEPTED IN THE NURSERY TRADE.
- ALL PLANT MATERIAL SHALL BE SPECIMEN QUALITY STOCK AS DETERMINED IN THE "AMERICAN STANDARDS FOR NURSERY STOCK" PUBLISHED BY THE AMERICAN ASSOCIATION OF NURSERYMEN, FREE OF PLANT DISEASES AND PESTS, OF TYPICAL GROWTH OF THE SPECIES AND HAVING A HEALTHY, NORMAL ROOT SYSTEM.
- SIZES INDICATED ON THE PLANT LIST ARE THE MINIMUM, ACCEPTABLE SIZE. IN NO CASE WILL SIZES LESS THAN THE SPECIFIED SIZES BE ACCEPTED.
- PLANTS SHALL NOT BE PRUNED PRIOR TO DELIVERY TO THE SITE OR AFTER INSTALLATION EXCEPT FOR THOSE BRANCHES THAT HAVE BEEN DAMAGED IN SOME WAY.
- PLANTS SHALL NOT HAVE NAME TAGS REMOVED PRIOR TO FINAL INSPECTION.
- ALL PLANTINGS SHALL RECEIVE A COMMERCIAL TRANSPLANT ADDITIVE PER MANUFACTURER'S RECOMMENDED RATES AND INSTRUCTIONS FOR APPLICATION.
- BUILDING MULCH SHALL BE 3" DEPTH OF KANSAS LARGE RIVER ROCK (2" SIZE). OTHER MULCH SHALL BE 3" OF DYED BROWN SHREDDED HARDWOOD. BOTH MULCHES SHALL BE OVER A FELT TYPE SOIL SEPARATOR CUT INTO THE GROUND WITH A TRENCHED EDGE. SEE TREE DETAIL FOR DIFFERENT MULCH AROUND TREES.
- SEE PLANTING DETAILS FOR SOIL MIX IN PLANTING HOLES.
- SOD SHALL BE A TURF-TYPE-TALL FESCUE GRASS BLEND.
- SUCCESSFUL LANDSCAPE BIDDER SHALL BE RESPONSIBLE FOR THE DESIGN AND INSTALLATION OF AN IRRIGATION SYSTEM TO BE APPROVED BY THE OWNER PRIOR TO CONSTRUCTION IF OWNER DESIRES.



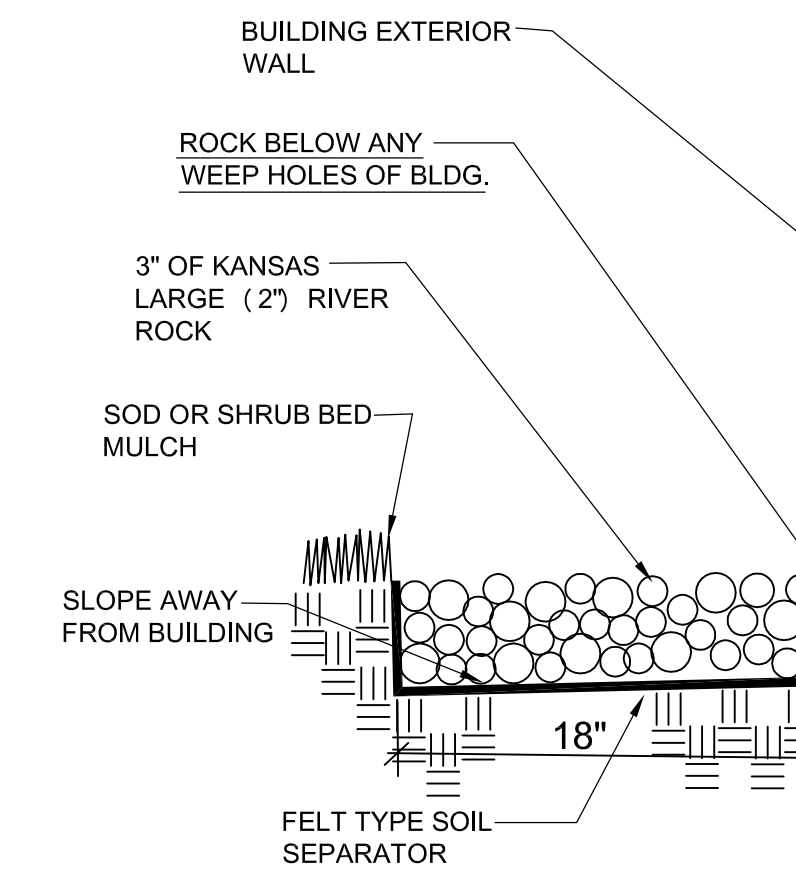
DIRECTION OF TREE STAKES:
ONE SOUTHEAST
ONE SOUTHWEST
ONE NORTH

PLASTIC SPIRAL TREE WRAP COIL FROM BASE TO LOWEST BRANCHES

WEBBED ARBOR TIE TAPE LOOP AROUND TREE TO BE 6"-8" LARGER THAN TRUNK DIAMETER



TREE PLANTING DETAIL
NO SCALE

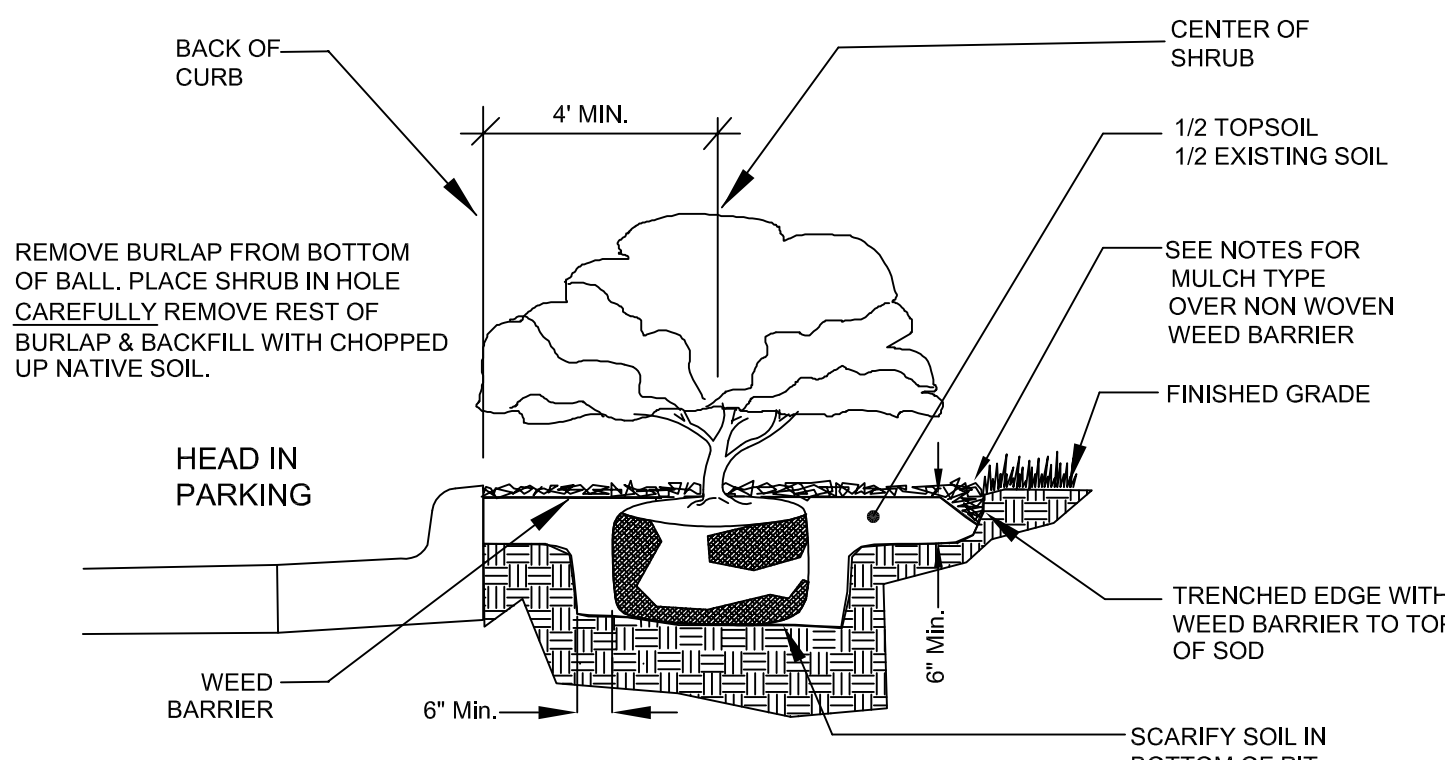


BUILDING ROCK EDGE
NO SCALE

*PLACE ROCK AROUND ENTIRE BLDG. PERIMETER WHEREVER THERE IS NOT CONCRETE OR ASPHALT

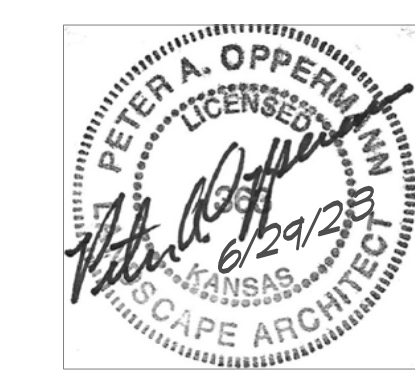
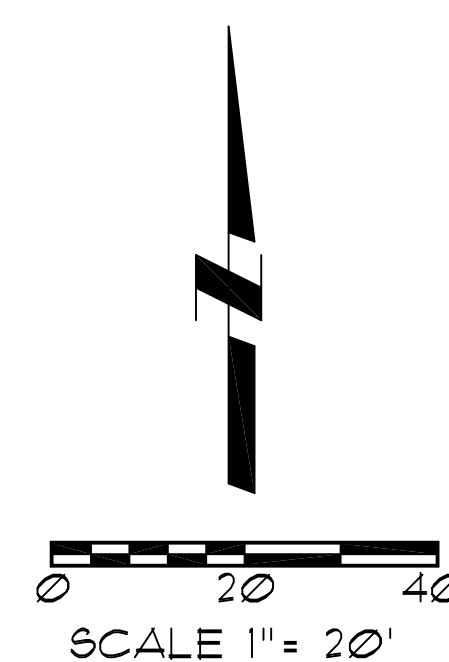
Transplant Additives:

- Apply a commercial transplant additive (approved by the Landscape Architect) to all trees, shrubs and groundcover at rates recommended by the manufacturer during the planting. This item shall be subsidiary to other planting items.
- Transplant additive shall be Horticultural Alliance "DIEHARD Transplant" (or approved equal) mycorrhizal fungal transplant inoculant or equivalent equal containing the appropriate species of mycorrhizal fungi and bacteria, fungi stimulant, water retaining agents, mineral & organic nutrients and inert ingredients.
- Demonstrate installation of all transplant additives for this project to the Landscape Architect. Provide actual additive product as evidence of sufficient quantity of product. (Empty product bags to be stockpiled for inspection by the Landscape Architect prior to disposal).
- Number of transplant additive packets per tree, shrub or groundcover shall be applied according to the manufacturer's recommended rates and instructions. For all plants the packet mix shall be evenly distributed into the upper approximately 8" of backfill soil next to the rootball. Do not place mix in the bottom of the planting pit.
- Furnishing and application of transplant additive shall be subsidiary to the planting operations.



SHRUB BED & PARKING SETBACK DETAIL
NO SCALE

Utility Note:
Utilities shown on plan are diagrammatic and some may be missing. Before starting any construction call appropriate locating service. In Kansas call 1-800-DIG-SAFE (344-7233) to have utilities located.



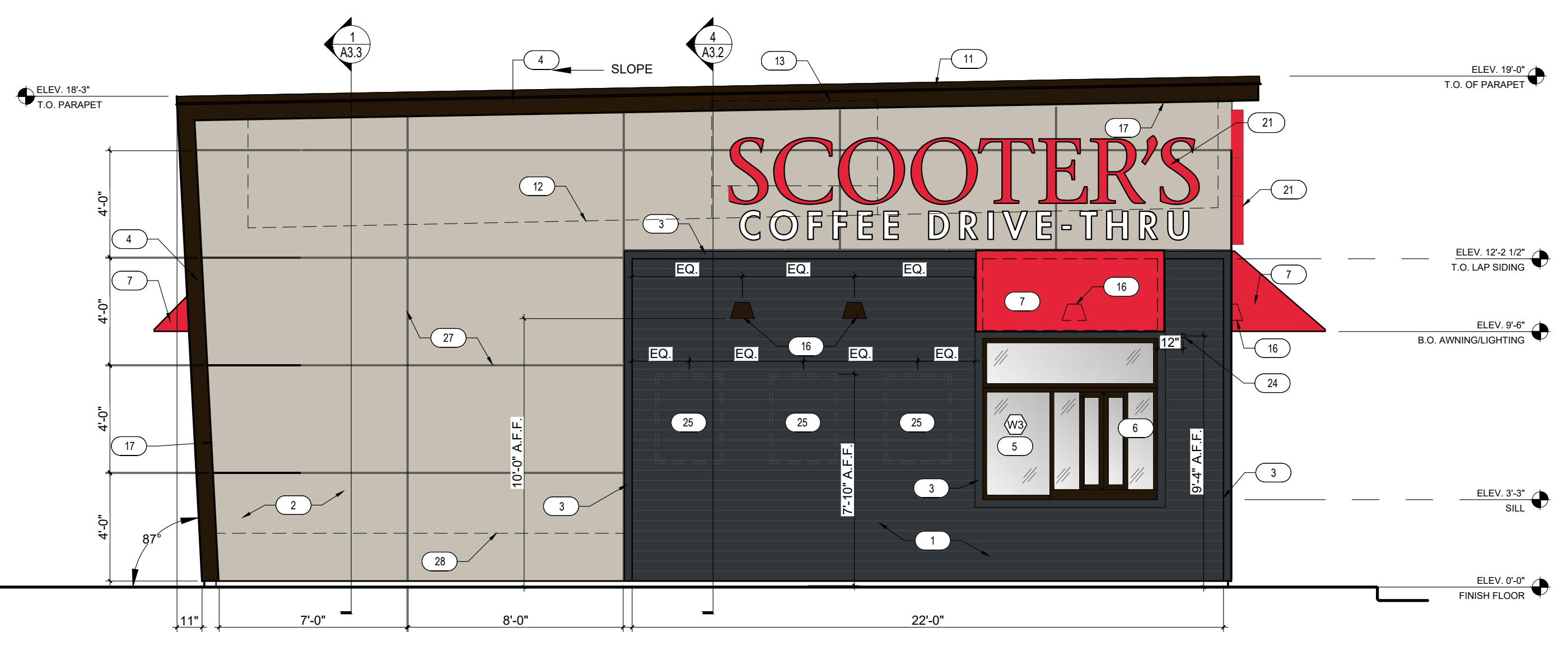
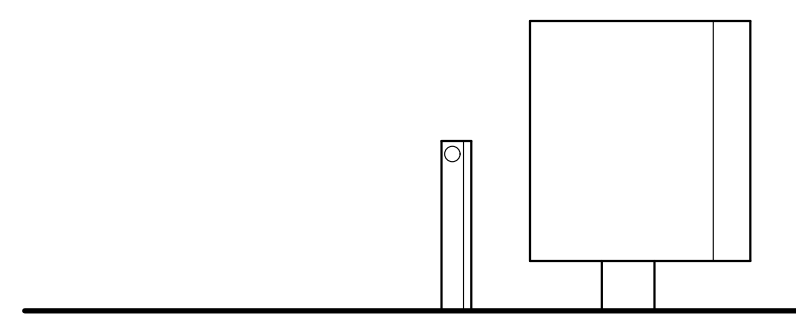
Landscape Plan
Scooter's
West Crestview Dr. and K-68 Hwy.
Louisburg, Kansas

Oppermann LandDesign, LLC
Land Planning & Landscape Architecture
22 Debra Lane petecoppermann56@gmail.com
New Windsor, New York 12553 913.529.5598

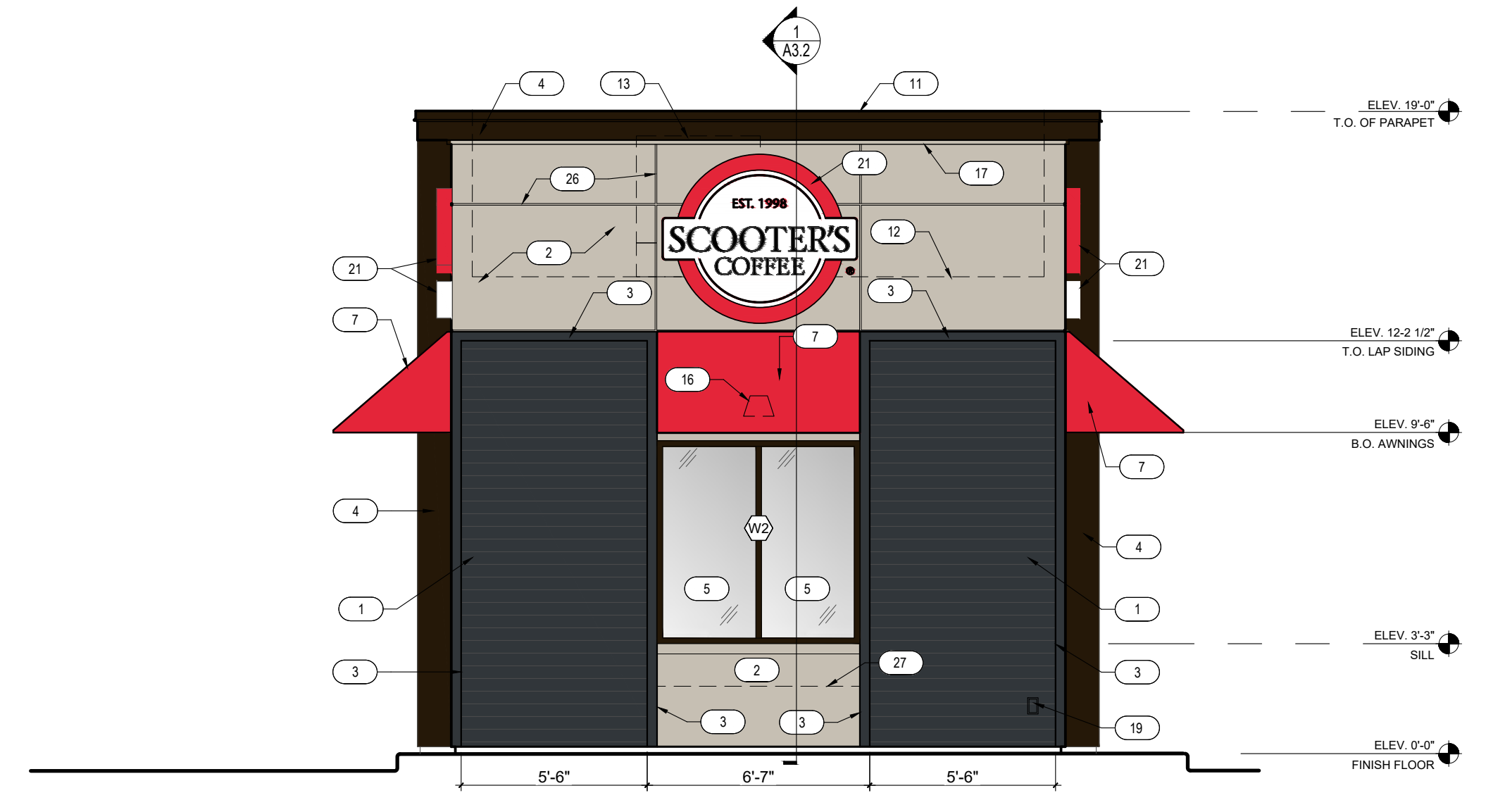
KEYNOTES

X

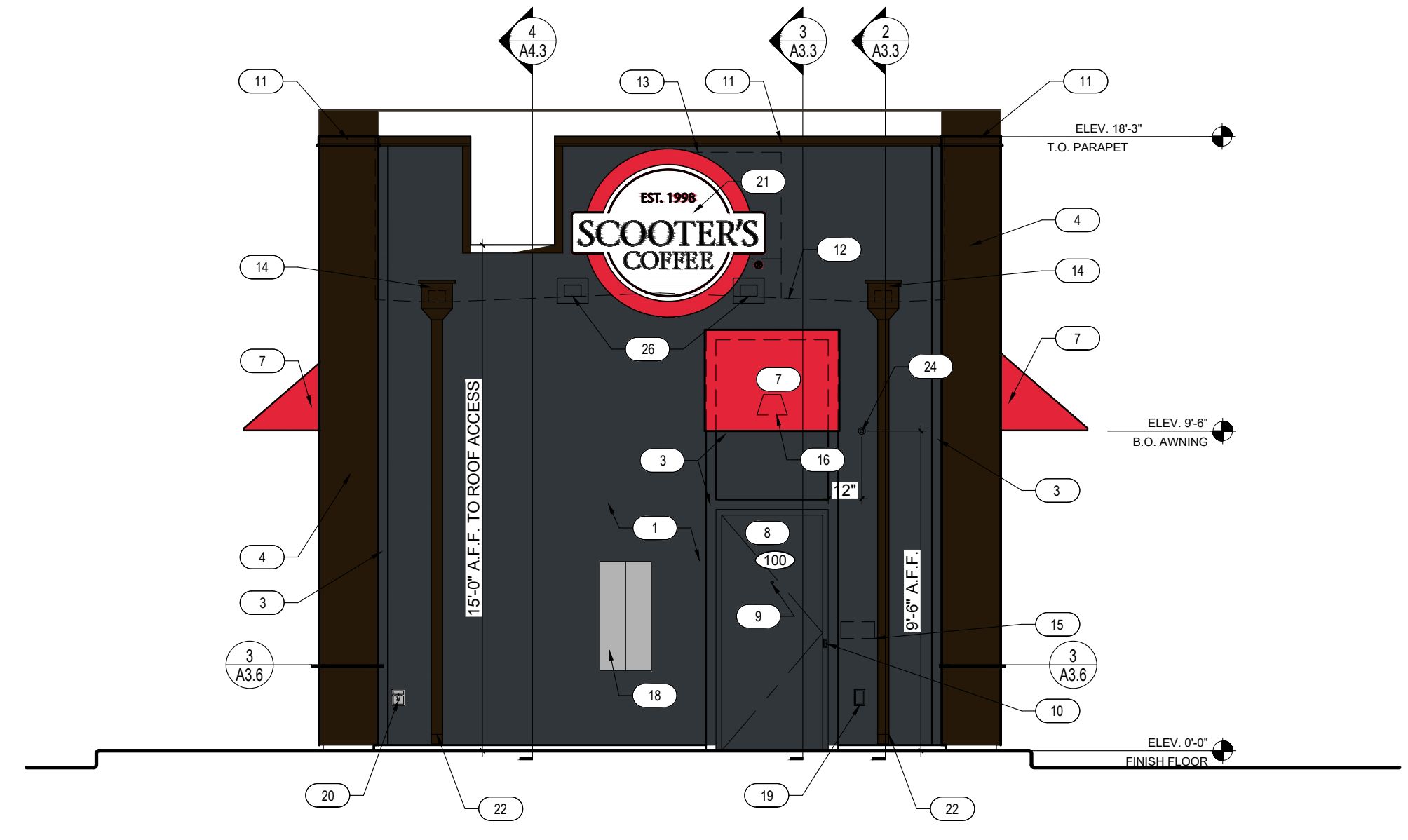
1. HARDIE PLANK HZ10 LAP SIDING CEDARMILL 6-1/4". SEE HARDIE DETAIL SHEET A3.6 - COLOR: SHERWIN WILLIAMS SW6992 INKWELL EGGHELL FINISH
2. EIFS W/ 1" RIGID INSULATION - SANDBLAST FINISH. SEE DETAIL SHEET A3.6 - COLOR: SW 1015 SKYLINE STEEL
3. 3 1/2" HARDIE TRIM, SEE HARDIE DETAIL SHEET A3.6 - COLOR: SHERWIN WILLIAMS SW6992 INKWELL EGGHELL FINISH
4. BRAKE METAL ACCENTS AND SOFFITS - COLOR: BLACK
5. INSULATED DARK BRONZE ALUMINUM WINDOWS WITH DUAL PANE TEMPERED GLASS
6. QUICKSERVE 48X48 WINDOW - COLOR: DARK BRONZE
7. AWNING BY OTHERS - COLOR: SUNBRELLA JOCKEY RED
8. INSULATED HOLLOW METAL DOOR AND FRAME - COLOR: SHERWIN WILLIAMS SW6992 INKWELL EGGHELL FINISH
9. WIDE ANGLE PEEP HOLE, BY DOOR MANUFACTURER
10. DOOR BELL
11. 22 GAUGE METAL PARAPET CAP
12. LINE OF ROOF BEYOND
13. ROOF TOP UNIT BEYOND, SEE MECHANICAL DRAWINGS
14. ROOF SCUPPER AND DOWNSPOUT, SEE DETAIL 8/A6.3
15. MAILBOX BY GC BLACK
16. WALL MOUNTED LIGHT FIXTURE, SEE ELECTRICAL DRAWINGS
17. LED LIGHT BAND, SEE ELECTRICAL DRAWINGS
18. SES PANEL, SEE ELECTRICAL DRAWINGS
19. ELECTRICAL OUTLETS, SEE ELECTRICAL DRAWINGS
20. HOSE BIBB, SEE PLUMBING DRAWINGS
21. PROPOSED SIGNAGE BY OTHERS, UNDER SEPARATE PERMIT
22. CONNECT DOWNSPOUTS TO UNDERGROUND PIPING, REF. CIVIL.
23. SPANDREL GLASS
24. NEW SECURITY CAMERA
25. CUSTOMER PROVIDED SIGN PANELS. PANELS TO BE MOUNTED TO FASCIA BY GC (WHEN PROVIDED) IN CUSTOMER SPECIFIED LOCATION USING PROPER HARDWARE AND FASTENERS (NON-CORROSIVE)



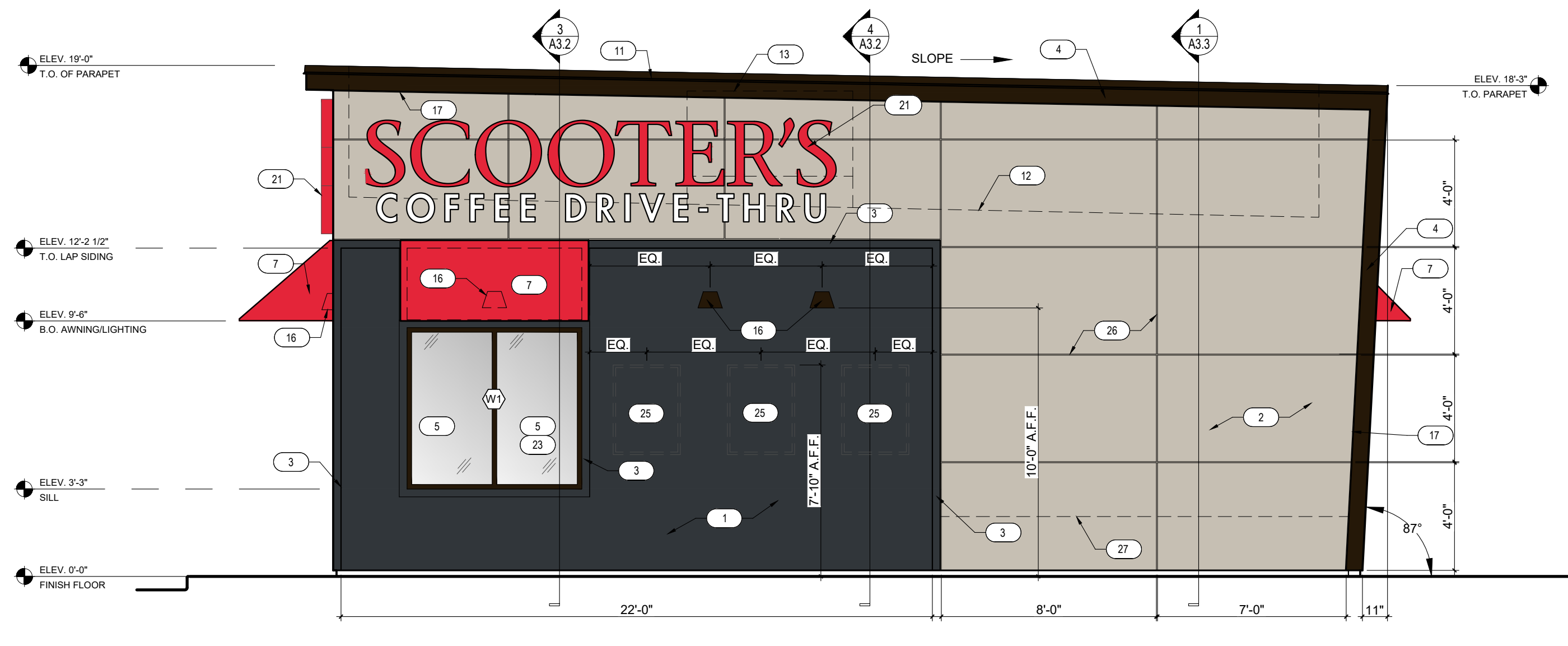
4 EXTERIOR ELEVATION - WEST
SCALE: 1/4" = 1'-0"



3 EXTERIOR ELEVATION - SOUTH
SCALE: 1/4" = 1'-0"



2 EXTERIOR ELEVATION - NORTH
SCALE: 1/4" = 1'-0"



1 EXTERIOR ELEVATION - EAST
SCALE: 1/4" = 1'-0"



PROJECT ADDRESS:
1500 W Amity St
Louisburg, KS

REVISIONS:

CLIENT:

Holy Grounds, LLC
dba Scooter's Coffee
11555 S Northwood Dr
Olathe, KS 66061

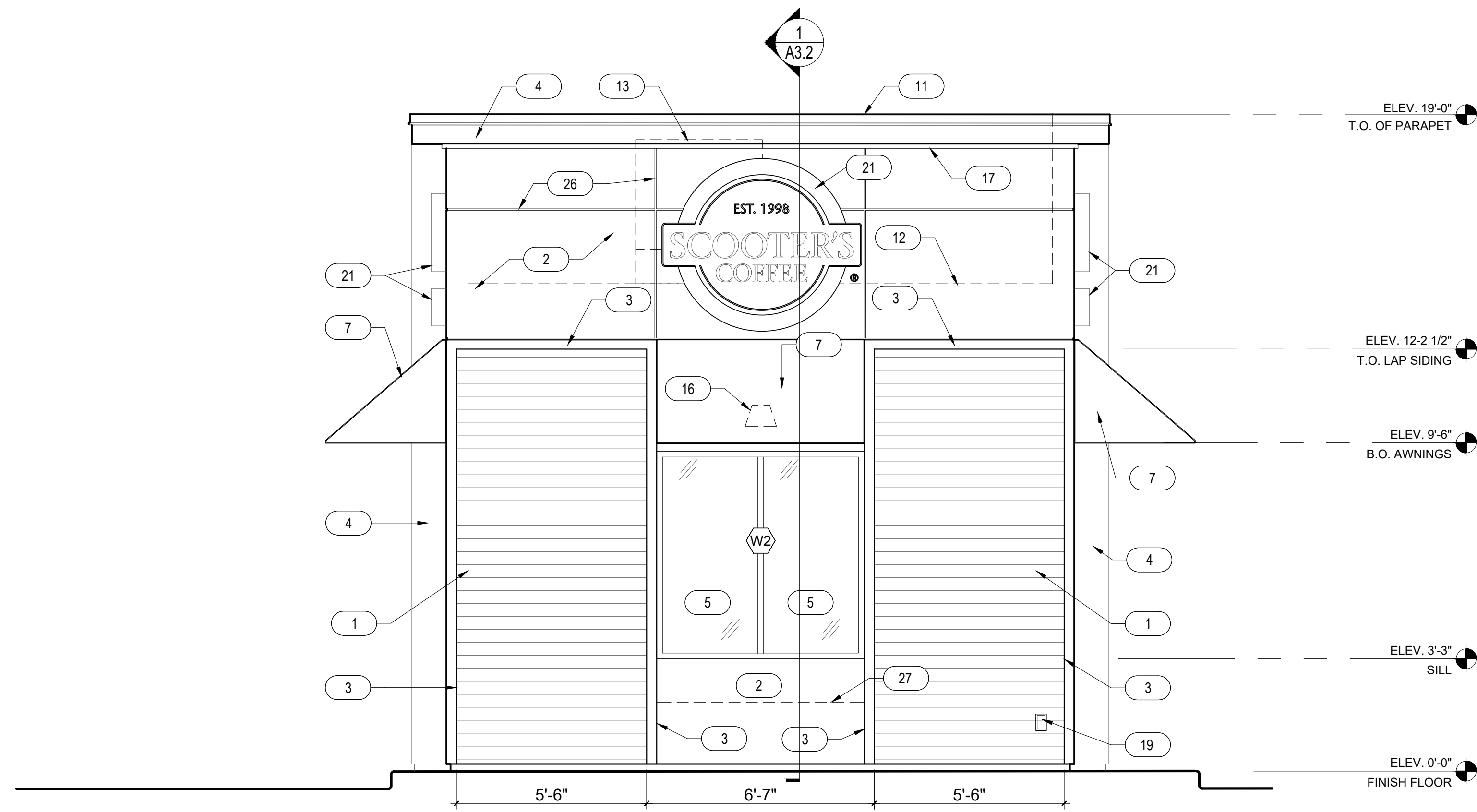
TITLE:
EXTERIOR
COLOR
ELEVATIONS

KIOSK PROTOTYPE:
4.0 PROTOTYPE
FEBRUARY 2022
DATE:
05/24/2023
PROJECT NO.
5964-23

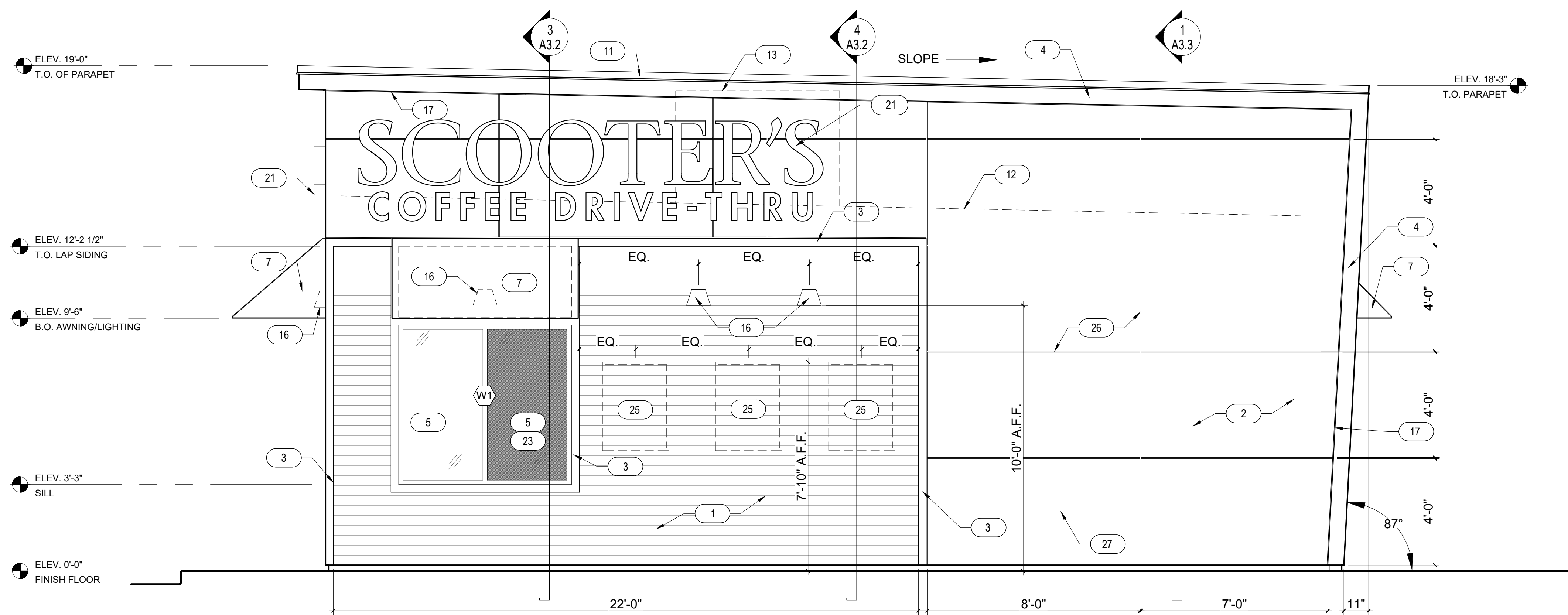
- PERMIT/BID SUBMITTAL
- CONSTRUCTION ISSUE

SHEET NO.

A2.0



1 EXTERIOR ELEVATION - SOUTH
SCALE: 3/8" = 1'-0"

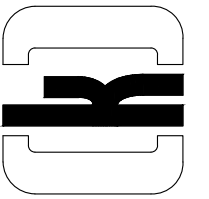


2 EXTERIOR ELEVATION - EAST
SCALE: 3/8" = 1'-0"

KEYNOTES X

1. HARDIE PLANK HZ10 LAP SIDING CEDAR MILL 6-1/4". SEE HARDIE DETAIL SHEET A3.6 - COLOR: SHERWIN WILLIAMS SW6992 INKWELL EGG SHELL FINISH
2. EIFS W/ 1" RIGID INSULATION - SANDBLAST FINISH, SEE DETAIL SHEET A3.6 - COLOR: SW 1015 SKYLINE STEEL
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9. WIDE ANGLE PEEP HOLE, BY DOOR MANUFACTURER
10. DOOR BELL
11. 22 GAUGE METAL PARAPET CAP
12. LINE OF ROOF BEYOND
13. ROOF TOP UNIT BEYOND, SEE MECHANICAL DRAWINGS
14. ROOF SCUPPER AND DOWNSPOUT, SEE DETAIL 8/A3.4
15. MAILBOX BY GC BLACK
16. WALL MOUNTED LIGHT FIXTURE, SEE ELECTRICAL DRAWINGS
17. LED LIGHT BAND, SEE ELECTRICAL DRAWINGS
18. SES PANEL, SEE ELECTRICAL DRAWINGS
19. ELECTRICAL OUTLETS, SEE ELECTRICAL DRAWINGS
20. HOSE BIBB, SEE PLUMBING DRAWINGS
21. PROPOSED SIGNAGE BY OTHERS, UNDER SEPARATE PERMIT
22. CONNECT DOWNSPOUTS TO UNDERGROUND PIPING, REF. CIVIL.
23. SPANDREL GLASS
24. NEW SECURITY CAMERA
25. CUSTOMER PROVIDED SIGN PANELS. PANELS TO BE MOUNTED TO FASCIA BY GC (WHEN PROVIDED) IN CUSTOMER SPECIFIED LOCATION USING PROPER HARDWARE AND FASTENERS (NON-CORROSIVE)
26. EIFS AESTHETIC REVEAL
27. HIGH-IMPACT MESH TO 24" AFF

WARMAN ARCHITECTURE+DESIGN
 1735 SWIFT AVE.
 NORTH KANSAS CITY, MISSOURI 64116
 V. 816.474.2233 F. 816.474.1051



PROJECT ADDRESS:
1500 W Amity St
Louisburg, KS

REVISIONS:

CLIENT:

Holy Grounds, LLC
dba Scooter's Coffee
11555 S Northwood Dr
Olathe, KS 66061

TITLE:

EXTERIOR ELEVATIONS

KIOSK PROTOTYPE:
4.0 PROTOTYPE
FEBRUARY 2022

DATE:

05/24/2023

PROJECT NO.

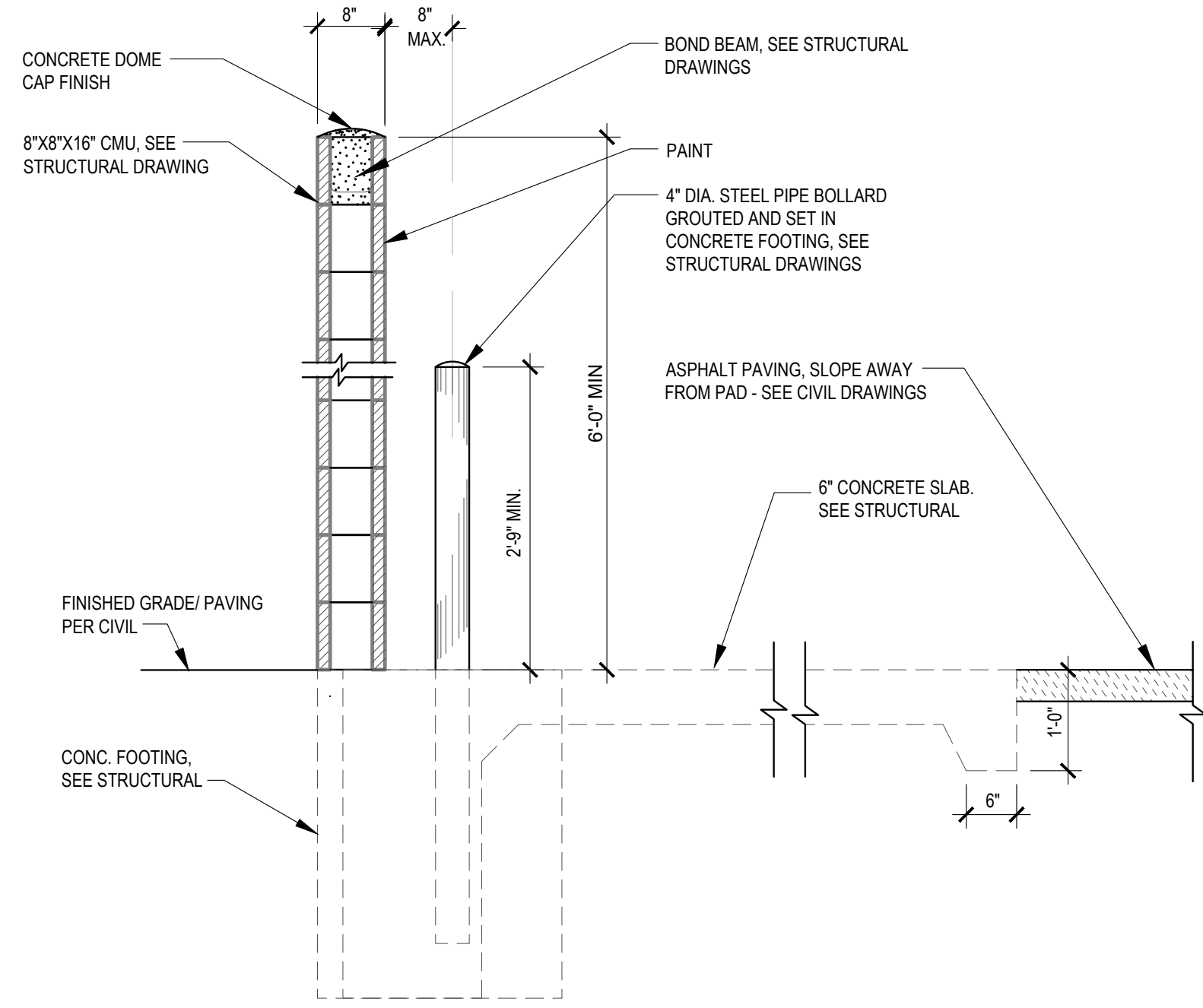
5964-23

PERMIT/BID SUBMITTAL

CONSTRUCTION ISSUE

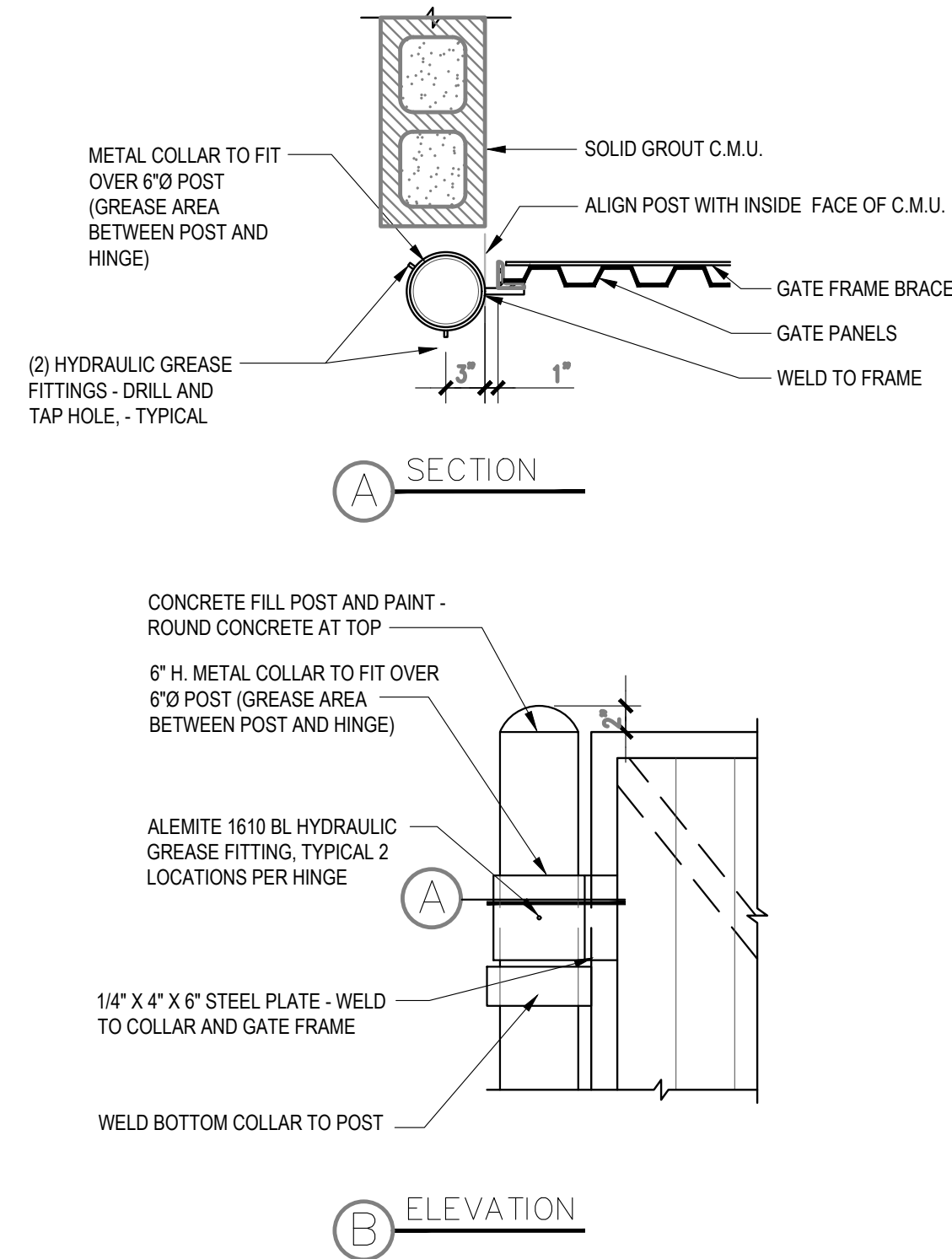
SHEET NO.

A2.1



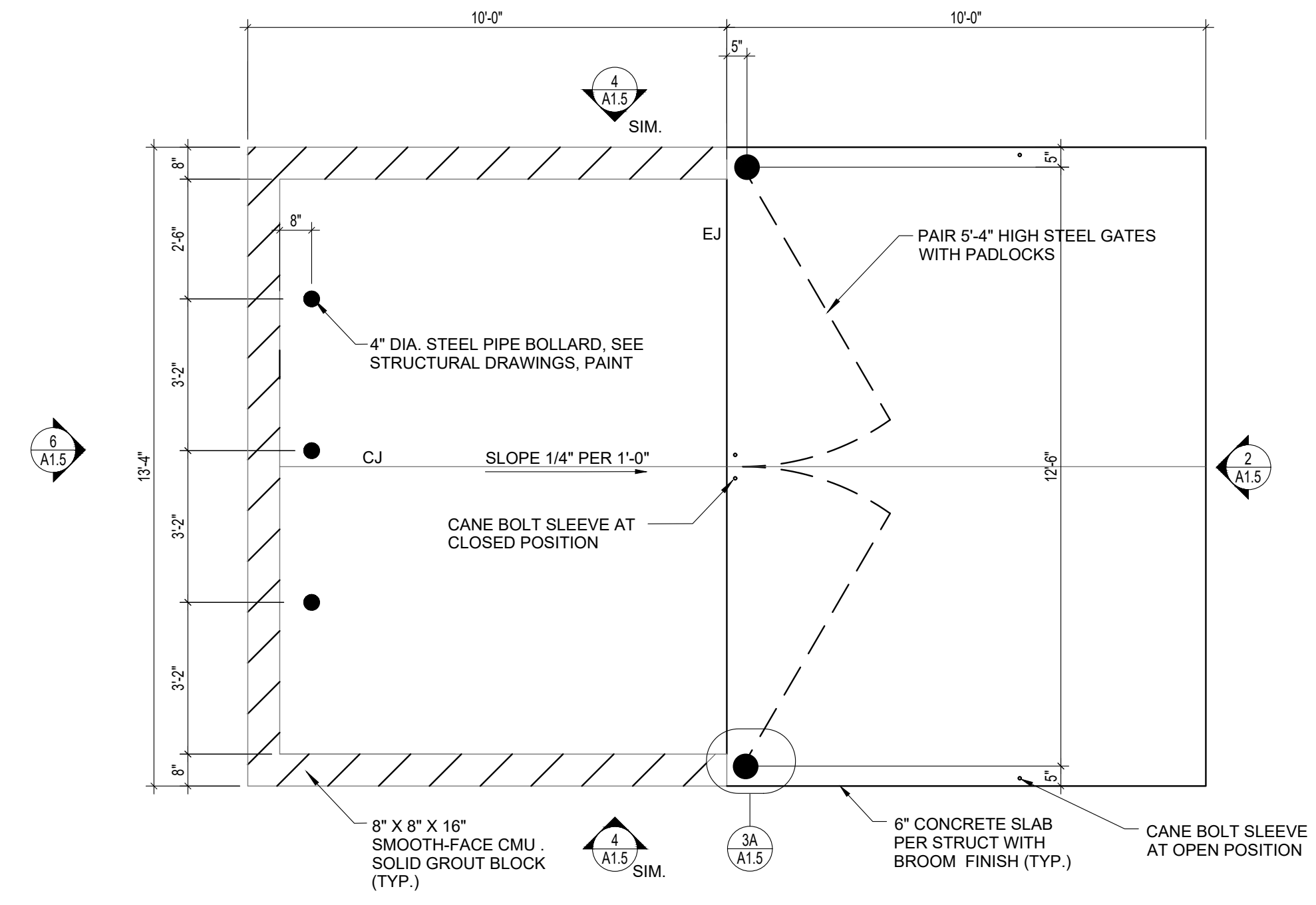
5 SECTION AT TRASH ENCLOSURE

SCALE: 3/4" = 1'-0"



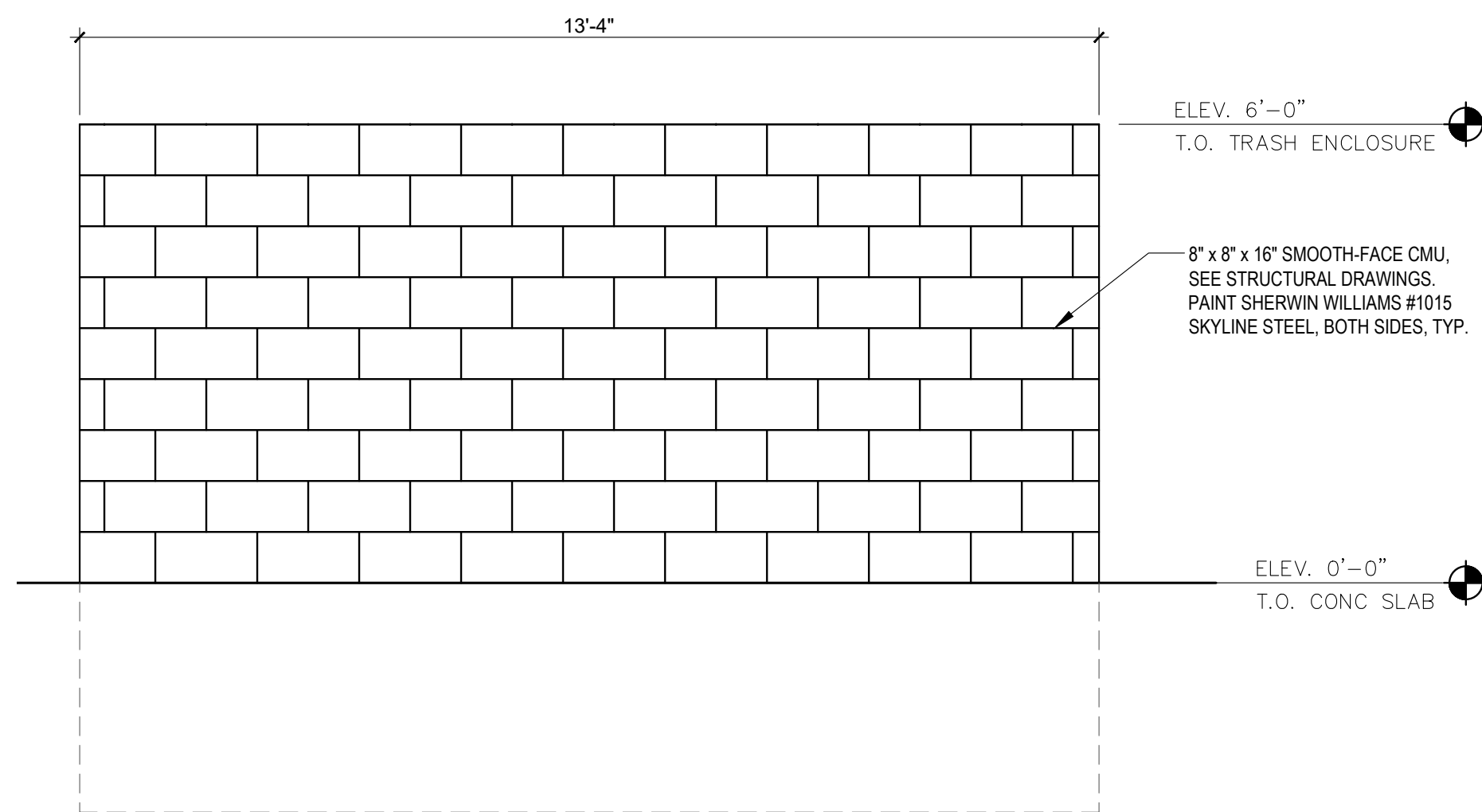
3 HINGE AT TRASH GATE

SCALE: NTS



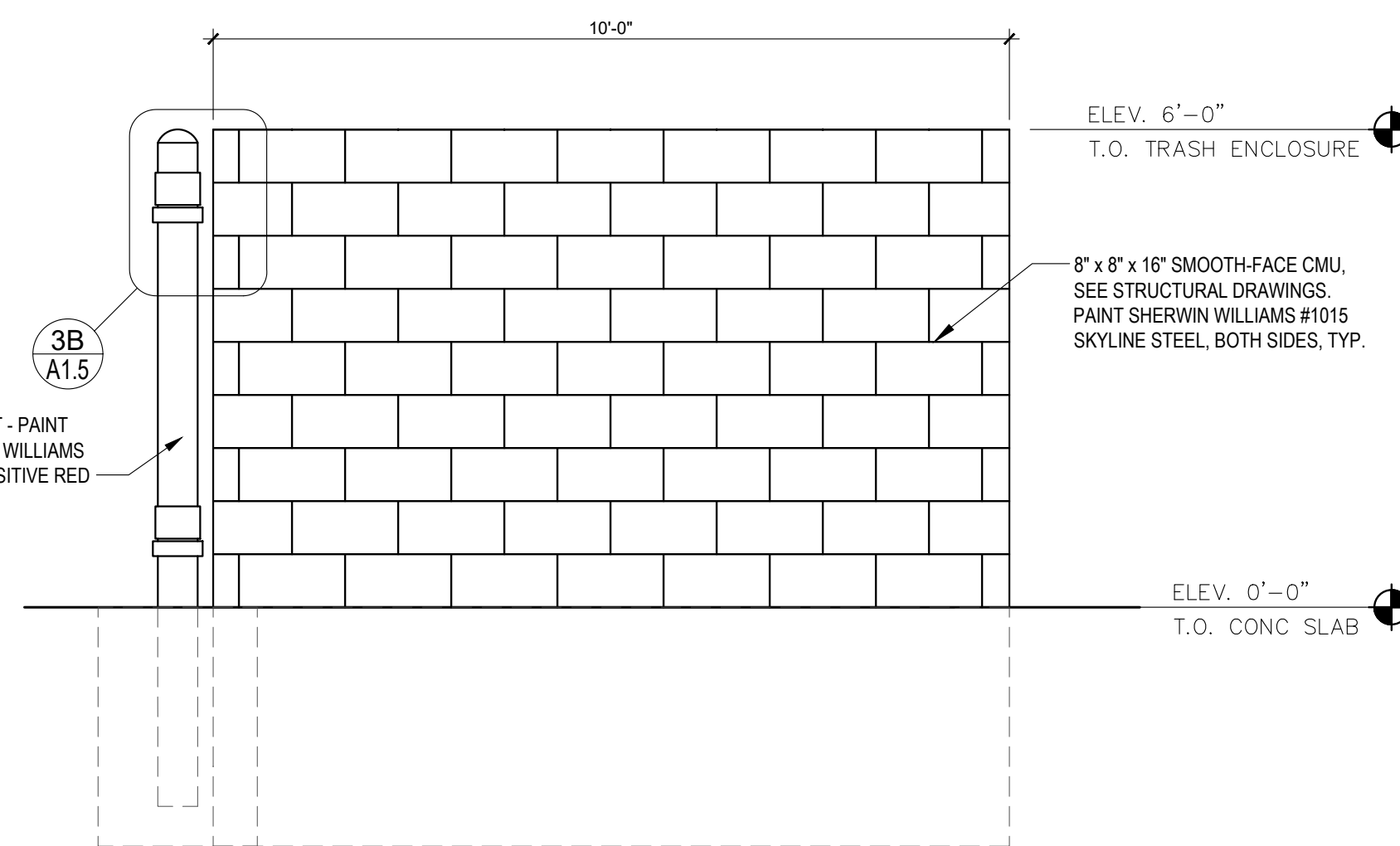
1 TRASH ENCLOSURE

SCALE: 3/8" = 1'-0"



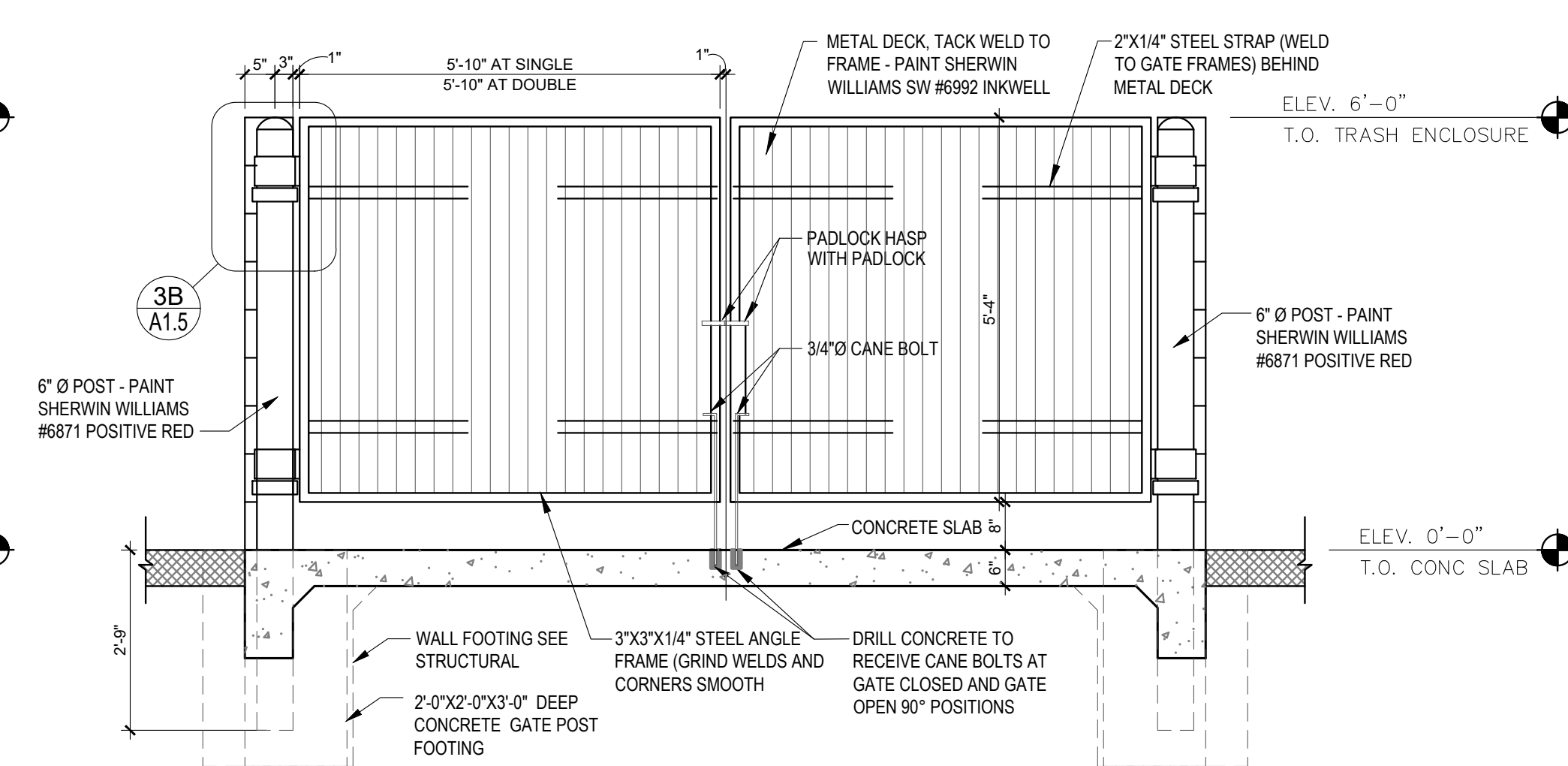
6 TRASH ENCLOSURE - BACK ELEVATION

SCALE: 1/2" = 1'-0"



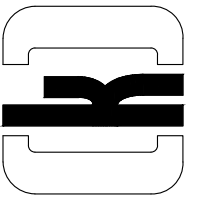
4 TRASH ENCLOSURE - SIDE BACK ELEVATION

SCALE: 1/2" = 1'-0"



2 TRASH ENCLOSURE - FRONT ELEVATION

SCALE: 1/2" = 1'-0"



PROJECT ADDRESS:
1500 W Amity St
Louisburg, KS

REVISIONS:

CLIENT:

Holy Grounds, LLC
dba Scooter's Coffee
11555 S Northwood Dr
Olathe, KS 66061

TITLE:
TRASH ENCLOSURE PLAN, ELEVATIONS & DETAILS

KIOSK PROTOTYPE:
4.0 PROTOTYPE
FEBRUARY 2022

DATE:

05/24/2023

PROJECT NO.

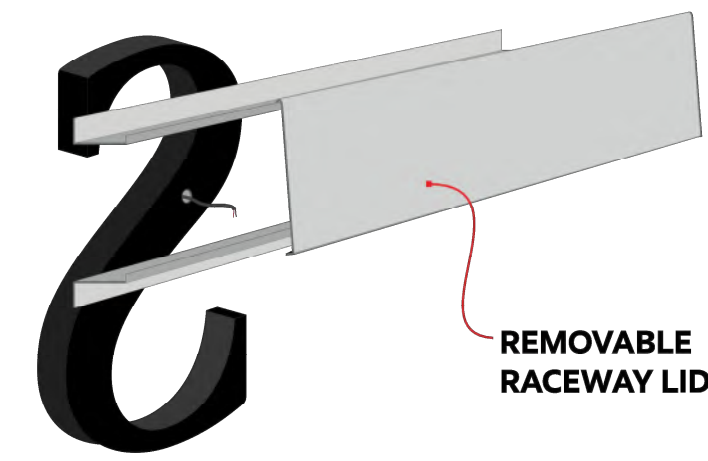
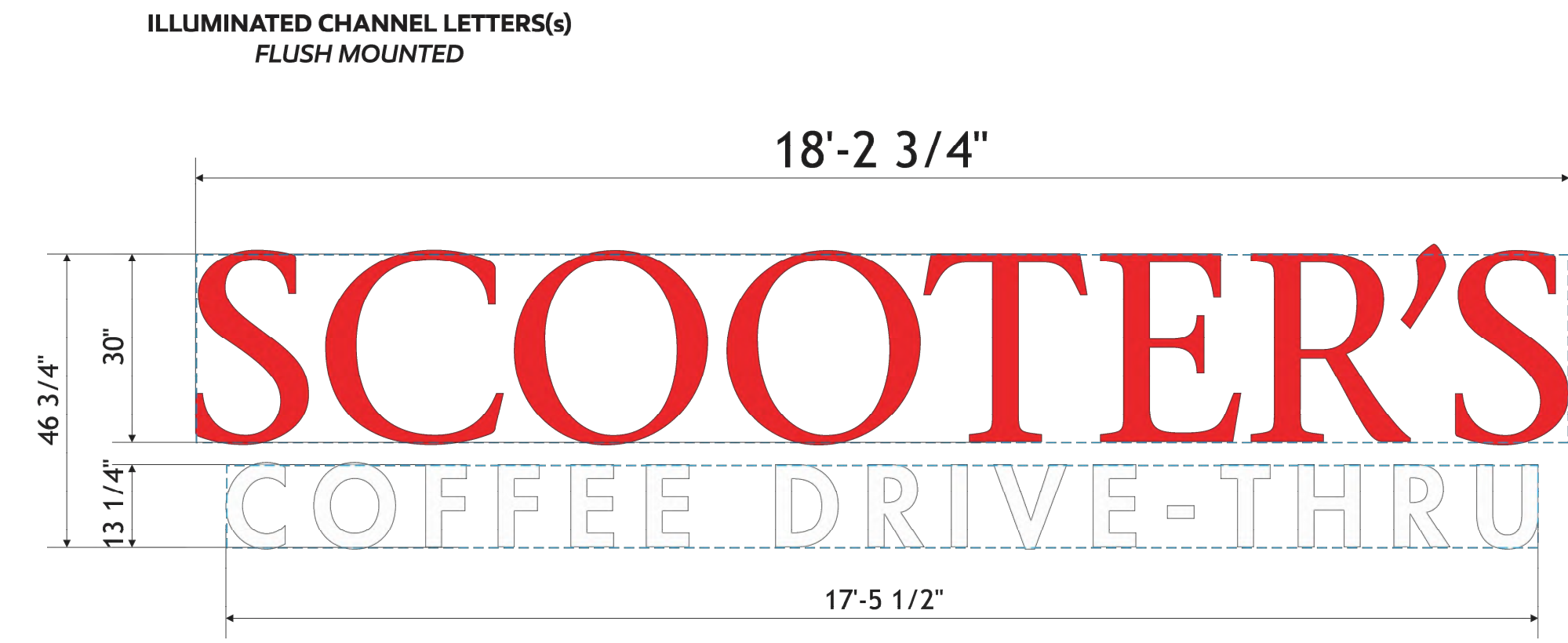
5964-23

PERMIT/BID SUBMITTAL

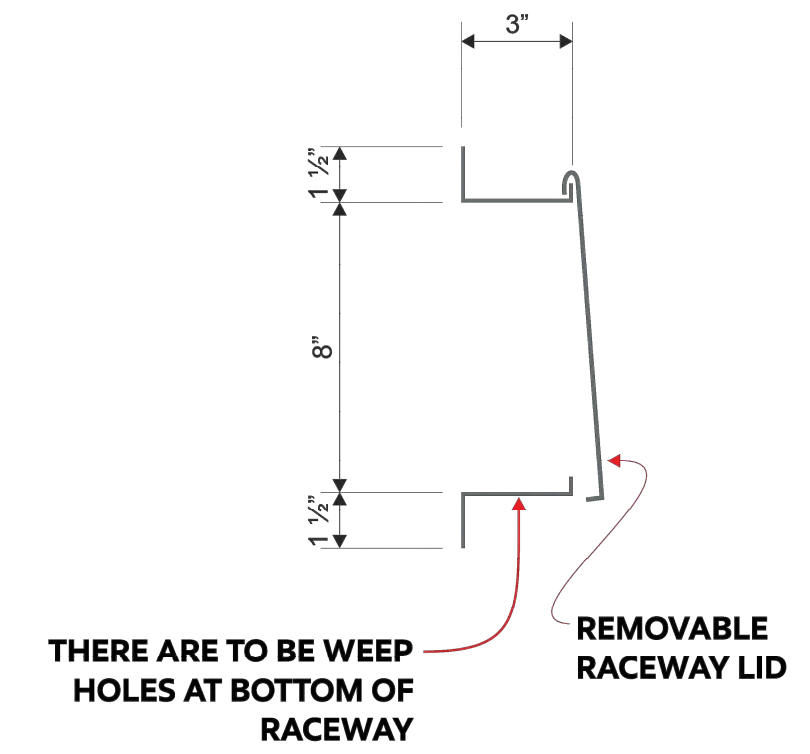
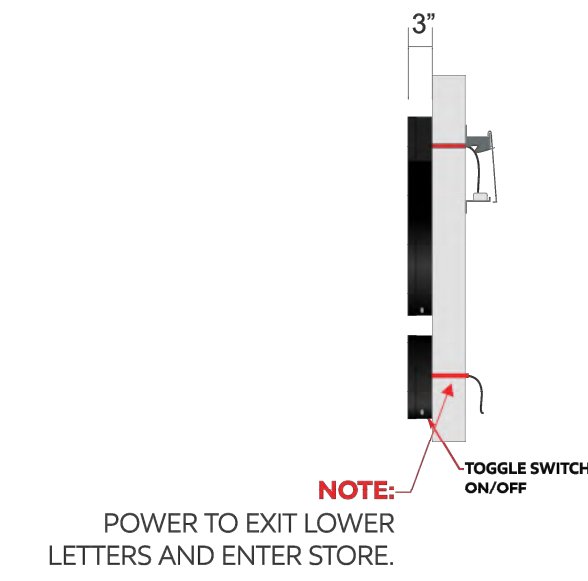
CONSTRUCTION ISSUE

SHEET NO.

A1.5

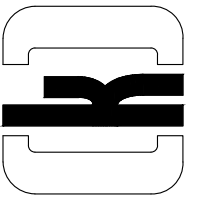


NOTE: INVERTED RACEWAY TO BE SEALED BY ROOFER AFTER INSTALLATION.



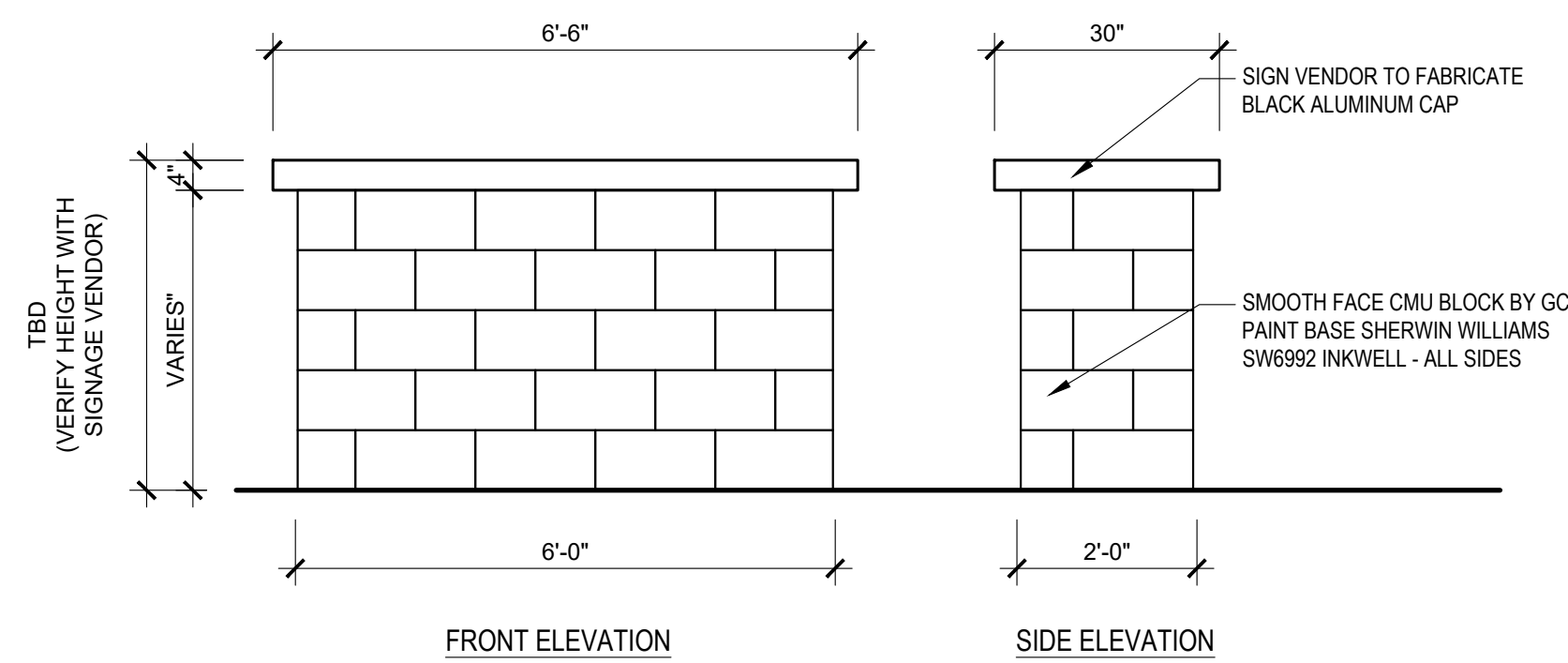
NOTE: SIGNAGE UNDER A SEPARATE PERMIT

4 INVERTED RACEWAY DETAIL
SCALE: NTS

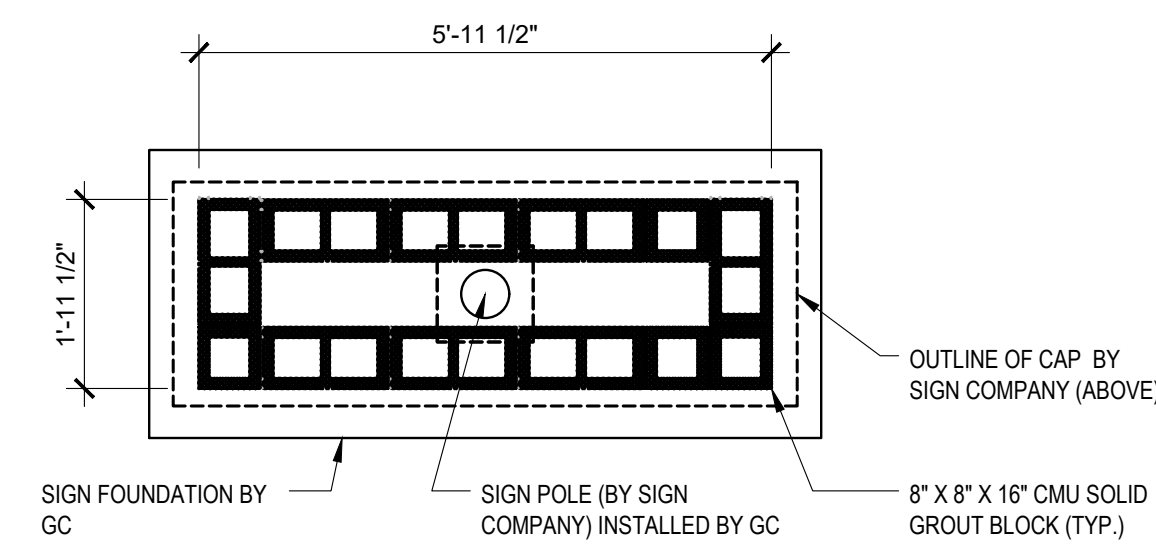


PROJECT ADDRESS:
1500 W Amity St
Louisburg, KS

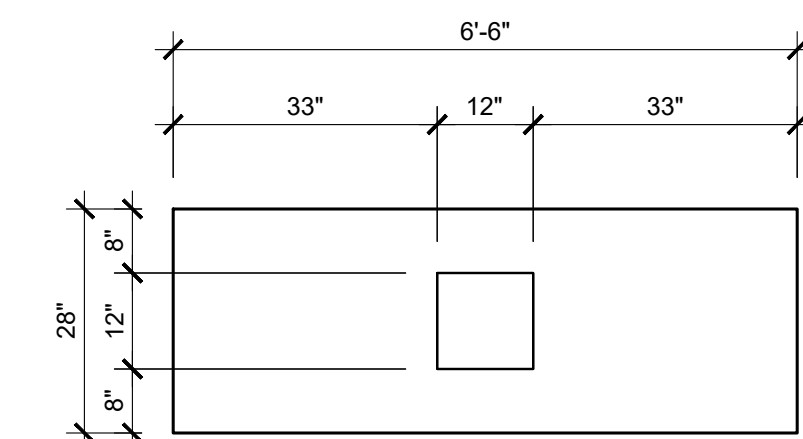
REVISIONS:



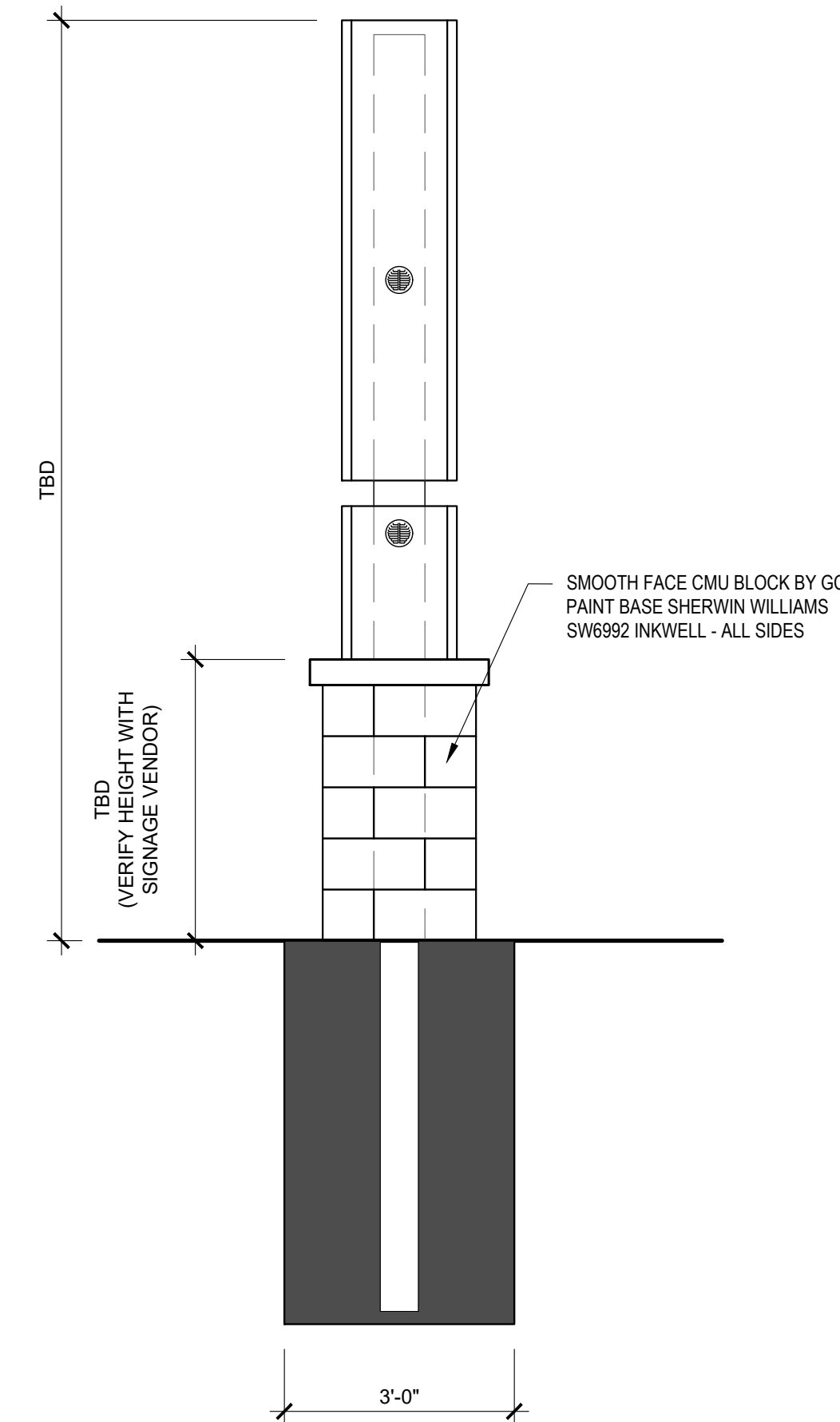
5 MONUMENT BASE ELEVATION
SCALE: 1/2" = 1'-0"



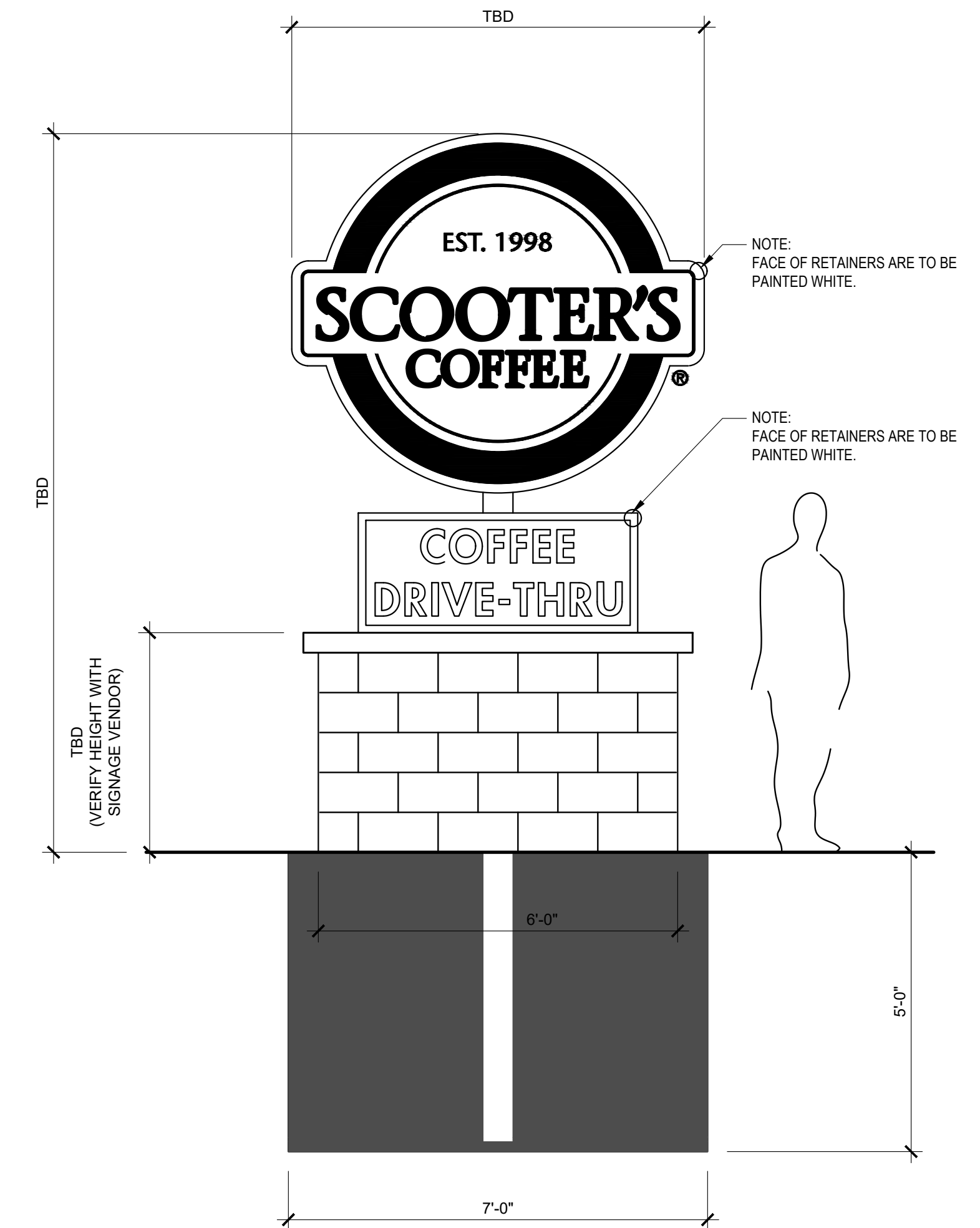
MONUMENT CAP - PLAN VIEW



3 MONUMENT CAP - PLAN VIEW
SCALE: 1/2" = 1'-0"



2 MONUMENT SIGN - SECTION
SCALE: 1/2" = 1'-0"



1 MONUMENT SIGN - FRONT ELEVATION
SCALE: 1/2" = 1'-0"

CLIENT:

Holy Grounds, LLC
dba Scooter's Coffee
11555 S Northwood Dr
Olathe, KS 66061

TITLE:
MONUMENT SIGN DETAILS
(FOR PRICING ONLY)

KIOSK PROTOTYPE:
4.0 PROTOTYPE
FEBRUARY 2022

DATE:
05/24/2023
PROJECT NO.
5964-23

PERMIT/BID SUBMITTAL
 CONSTRUCTION ISSUE

SHEET NO.

Item #7
23001-Z (Rezoning)
26,100 square foot tract of vacant land.
7 West Amity Parcel ID: 1093101001002000

Owner: City of Louisburg
Applicant: City of Louisburg
Meeting Date: July 26, 2023

The City of Louisburg wishes to rezone this property from “R-1” Single-Family Dwelling to “C-3” General Business District. Staff has determined that rezoning this property would be consistent with the Bright Future Comprehensive Plan dated December 4, 2017. This plan identifies this property as “NCC” Neighborhood Corridor Commercial.

The below web address will direct Planning Commission members to the Bright Future Comprehensive Plan:

<https://louisburgkansas.gov/DocumentCenter/View/366/Louisburg-Comprehensive-Plan-12-4-17>

Adjacent Property Zoning:

North – “C-3” General Business District (MMB, LLC and Landmark Bank)

South – “R-1” Single-Family Residential

East – “C-3” General Business District (Auburn Pharmacy)

West – “R-1” Single-Family Residential (Sonic has SUP)



The Planning Commission should determine if the property should be rezoned from “R-1” to “C-3”.

The City has provided written notice of this public hearing to all property owners within the required 200-foot radius of the subject property. Staff has also placed a public notice in newspaper to advertise this hearing. This ad appeared in the newspaper on July 5, 2023. To date Staff has not received any comments from the neighbors concerning this proposal.

All Planning Commission recommendations will be forwarded to the City Council for additional review and consideration. This item with Planning Commission recommendation will be presented to the City Council on August 21, 2023. Property owners within 200 feet of subject property described above, who would like to protest the proposed rezone, will have fourteen (14) days after the Planning Commission makes its recommendation to submit a protest petition to the office of the City of Louisburg City Clerk. If a valid protest petition is filed with the City Clerk prior to the deadline, the change shall require approval by a 3/4 majority vote of the City Council. Protest petitions are available in the office of the Codes Administrator for the City of Louisburg.

NOTE:

This parcel was at one time four separate parcels of land (Parcel ID: 1093101001004000, 1903101001003000, 1093101001005000, and 1093101001002000). All four parcels were acquired by the City of Louisburg on July 30, 2014, and merged into one parcel on January 7, 2019.

This property is legally described as the following:

All that part of Block "N" in the City of Louisburg, Miami County, Kansas described as follows:

Commencing at the Northeast corner of the Northeast Quarter of Section 31, Township 16 South, Range 25 East, Miami County, Kansas thence S88°06'35"W, along the North line of the Northeast Quarter of said Section 31, a distance of 215.00 feet; thence S2°07'39"E, parallel to the East line of the Northeast Quarter of said Section 31, a distance of 40.00 feet to a point on the South right of way line of Highway 68, said point being the Point of Beginning, thence S2°07'39"E, parallel to the East line of the Northeast Quarter of said Section 31, a distance of 90.00 feet; thence S88°06'35"W, parallel to the North line of the Northeast Quarter of said Section 31, a distance of 289.96 feet, to a point on the East right of way line of Vine Street; thence N2°10'34"W, along said right of way line, a distance of 90.00 feet, to a point on the South right of way line of said Highway 68; thence N88°06'35"E, along said right of way line, a distance of 290.03 feet to the Point of Beginning, containing 0.06 Acres more or less.

Eight Golden Factors to Consider

In 1978, the Supreme Court of Kansas handed down a decision in the Golden vs. City of Overland Park case that established the basis for considering and reaching a decision on zoning changes. The decision should be made based on the evidence submitted and the factors considered. The eight Golden Factors emerged from this case and have become institutionalized in the consideration of zoning changes in Kansas. A more recent court case determined that Special Use Permits were, in reality, a land use change and should be considered by the same factors.

The factors must be considered by the Planning Commission and Governing Body in order to determine whether granting or denying a zoning change, including a Special Use Permit, is reasonable. It is not necessary that findings on all the factors be favorable to approve or be unfavorable to deny the zoning change or Special Use Permit. Also, not all the factors carry the same weight and the weight may vary from case to case.

1. **The character of the neighborhood:** Factual description of the application area and surrounding property as to land uses, density, intensity, general condition, age of structure, etc.
2. **The zoning and uses of property nearby:** Factual listing of the zoning surrounding the property along with the specific abutting uses.
3. **The suitability of the property for the uses to which it has been restricted under its existing zoning:** How is the property currently zoned and what uses are allowed on the property? Are these uses suitable given surrounding zoning and site criteria? Are the current allowed uses the only ones which might be appropriate for this property?
4. **Extent to which removal of the restrictions will detrimentally affect nearby property:** Can the uses allowed in the requested district be good neighbors to existing development? This is a subjective question. The focus should be on facts, not fears, and should be based on issues that zoning can address (e.g., allowed uses, minimum lot sizes, height, setbacks, traffic, etc.)
5. **Length of time of any vacancy of the property:** Factual information, but its importance can be somewhat subjective. A property might be vacant because the current zoning is unsuitable, but there may be other reasons not related to zoning. Some examples might be a glut of available property of the same zoning district, financing problems, speculation, lack of available services or other development problems.
6. **Relative gain to the public health, safety and welfare by destruction of value of the applicant's property as compared to the hardship on other individual landowners:** The protection of the public health, safety and welfare is the basis for zoning. The relationship between the property owner's right to use and obtain value from their property and the City's responsibility to its citizens should be weighed.
7. **Recommendation of professional staff:** Should be based on the evidence presented, the factors, adopted plans and policies, and other technical reports (e.g., Capital Improvement Programs, facility master plans, etc.) which speak to the topic and staff's best professional judgment.

8. Conformance with the Comprehensive Plan: Does the request agree with the adopted plan recommendations? If not, is the plan out-of-date or are there mitigating circumstances which speak to the nonconformity?



Memo:

To: Louisburg Planning Commission

From: City Staff

Date: July 24, 2023

Re: Presentation of proposed plot plan

Scheduled visitor Joel LaJoie, developer for the proposed redevelopment of the former Louisburg Golf Course, wishes to address the Planning Commission regarding the conceptual site plan of the project.

Joel LaJoie and Brian Smith approached the Planning Commission at the June meeting to discuss the concept of the development during public comments.

Mr. LaJoie would like to secure feedback from the Planning Commission on this conceptual plan before moving forward with the preliminary plat plan at a future meeting.

Financial: None.

Recommendation: Review and discuss.

Site Data:	
75x150 Lots	85
100x150 Lots	141
Total Lots	226

Lots and ROW	119.7
Open Space/BMP's	81.0
Total Acres	200.7

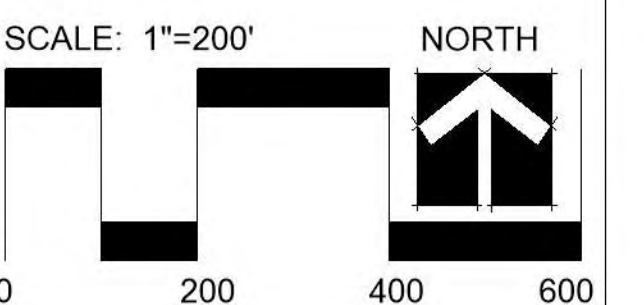
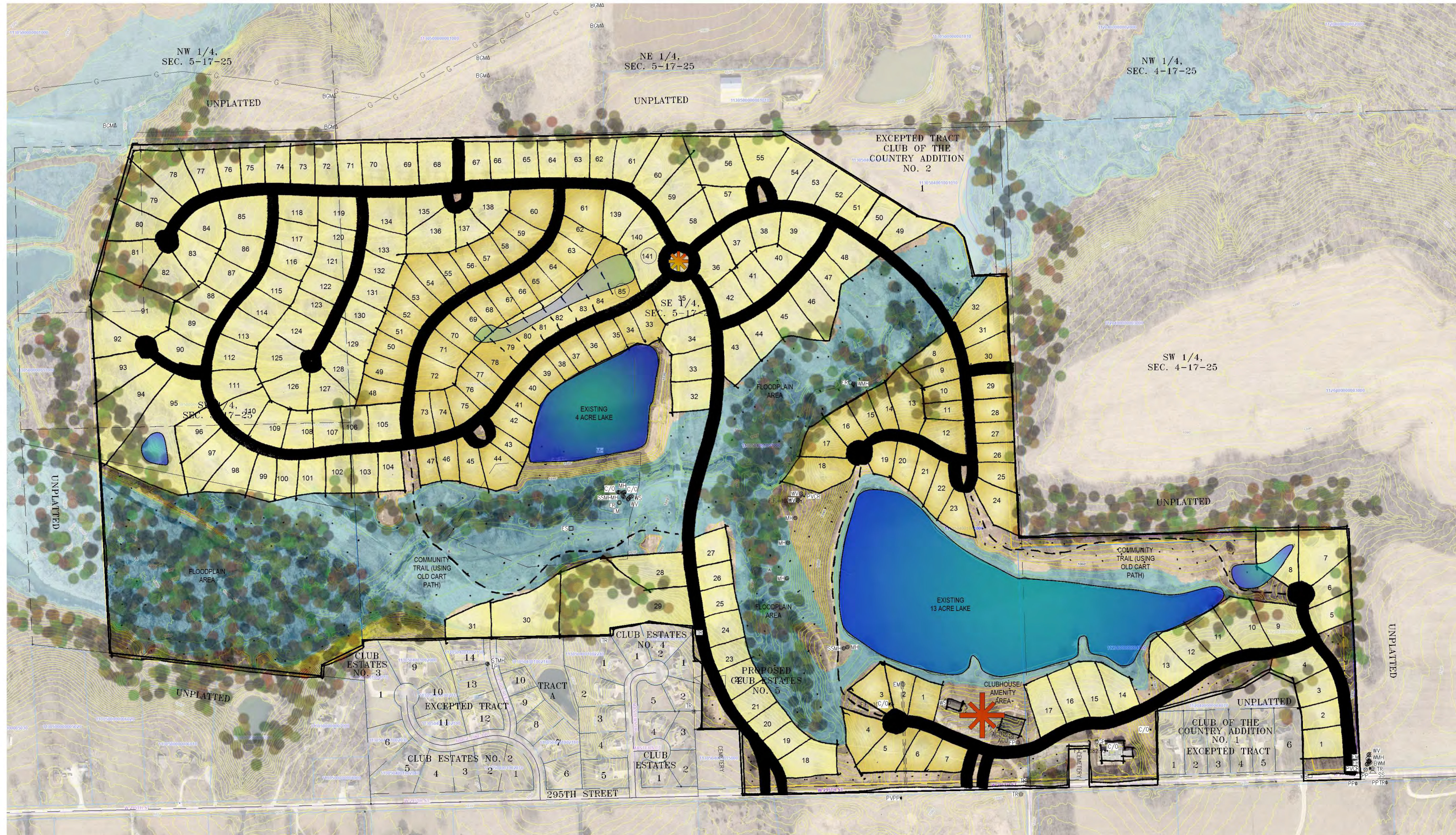
1.1 DU/AC
17,045 LF of local streets (28' wide)

MEIER
LANDSCAPE
ARCHITECTURE
15245 Metcalf Ave.
Overland Park, KS 66223
913.787.2817



CLIENT
Tom Smith
Louisburg KS

PROJECT
Golf Course
Redevelopment Site
Plan Design
295th Street
Louisburg KS



Date: 7.25.2023
Project #: 1050
Conceptual Site Plan



Memo:

To: Louisburg Planning Commission
From: City Staff
Date: July 21, 2023
Re: Annual Comprehensive Plan Review

In accordance with Kansas Statute 12-747, the Planning Commission shall review or reconsider the Comprehensive Plan, at least once a year.

Louisburg City Council adopted the Bright Future Comprehensive Plan on December 4, 2017. The Planning Commission will review Part 2: Comprehensive plan/Community Vision and Land Use Strategy Plan. See attached documents.

The entire plan can be viewed at
<https://www.louisburgkansas.gov/DocumentCenter/View/366/Louisburg-Comprehensive-Plan-12-4-17>

Financial: None.

Recommendation: Please review the attached documents and discuss.

2019 Kansas Statutes

12-747. Same; comprehensive plan; contents; procedure for adoption; annual review of plan. (a) A city planning commission is hereby authorized to make or cause to be made a comprehensive plan for the development of such city and any unincorporated territory lying outside of the city but within the same county in which such city is located, which in the opinion of the planning commission, forms the total community of which the city is a part. The city shall notify the board of county commissioners in writing of its intent to extend the planning area into the county. A county planning commission is authorized to make or cause to be made a comprehensive plan for the coordinated development of the county, including references to planning for cities as deemed appropriate. The provisions of this subsection may be varied through interlocal agreements.

(b) The planning commission may adopt and amend a comprehensive plan as a whole by a single resolution, or by successive resolutions, the planning commission may adopt or amend parts of the plan. Such resolution shall identify specifically any written presentations, maps, plats, charts or other materials made a part of such plan. In the preparation of such plan, the planning commission shall make or cause to be made comprehensive surveys and studies of past and present conditions and trends relating to land use, population and building intensity, public facilities, transportation and transportation facilities, economic conditions, natural resources and may include any other element deemed necessary to the comprehensive plan. Such proposed plan shall show the commission's recommendations for the development or redevelopment of the territory including: (a) The general location, extent and relationship of the use of land for agriculture, residence, business, industry, recreation, education, public buildings and other community facilities, major utility facilities both public and private and any other use deemed necessary; (b) population and building intensity standards and restrictions and the application of the same; (c) public facilities including transportation facilities of all types whether publicly or privately owned which relate to the transportation of persons or goods; (d) public improvement programming based upon a determination of relative urgency; (e) the major sources and expenditure of public revenue including long range financial plans for the financing of public facilities and capital improvements, based upon a projection of the economic and fiscal activity of the community, both public and private; (f) utilization and conservation of natural resources; and (g) any other element deemed necessary to the proper development or redevelopment of the area. Before adopting or amending any such plan or part thereof, the planning commission shall hold a public hearing thereon, notice of which shall be published at least once in the official city newspaper in the case of a city or in the official county newspaper in the case of a county. Such notice shall be published at least 20 days prior to the date of the hearing. Upon the adoption or amendment of any such plan or part thereof by adoption of the appropriate resolution by a majority vote of all members of the planning commission, a certified copy of the plan or part thereof, together with a written summary of the hearing thereon, shall be submitted to the governing body. No comprehensive plan shall be effective unless approved by the governing body as provided by this section. The governing body either may: (1) Approve such recommendations by ordinance in a city

or resolution in a county; (2) override the planning commission's recommendations by a 2/3 majority vote; or (3) may return the same to the planning commission for further consideration, together with a statement specifying the basis for the governing body's failure to approve or disapprove. If the governing body returns the planning commission's recommendations, the planning commission, after considering the same, may resubmit its original recommendations giving the reasons therefor or submit new and amended recommendations. Upon the receipt of such recommendations, the governing body, by a simple majority thereof, may adopt or may revise or amend and adopt such recommendations by the respective ordinance or resolution, or it need take no further action thereon. If the planning commission fails to deliver its recommendations to the governing body following the planning commission's next regular meeting after receipt of the governing body's report, the governing body shall consider such course of inaction on the part of the planning commission as a resubmission of the original recommendations and proceed accordingly. The comprehensive plan and any amendments thereto shall become effective upon publication of the respective adopting ordinance or resolution.

(c) An attested copy of the comprehensive plan and any amendments thereto shall be sent to all other taxing subdivisions in the planning area which request a copy of such plan. Such plan or part thereof shall constitute the basis or guide for public action to insure a coordinated and harmonious development or redevelopment which will best promote the health, safety, morals, order, convenience, prosperity and general welfare as well as wise and efficient expenditure of public funds.

(d) At least once each year, the planning commission shall review or reconsider the plan or any part thereof and may propose amendments, extensions or additions to the same. The procedure for the adoption of any such amendment, extension or addition to any plan or part thereof shall be the same as that required for the adoption of the original plan or part thereof.

History: L. 1991, ch. 56, § 7; L. 1997, ch. 147, § 4; May 1.

GOALS AND POLICY STATEMENTS

Goals	Policies for Decision-Makers / The City's Role
Foster a diverse mix of land uses that support and grows Louisburg's tax base.	<p>Balance land uses by pursuing a mix of residential uses and housing types.</p> <p>Introduce and encourage employment-generating land uses that enhance Louisburg's local economy.</p> <p>Concentrate commercial uses in the Traditional Downtown and along the Amity Street and Metcalf Road corridors.</p>
Establish Downtown Louisburg as a vibrant and vital mixed-use center.	<p>Encourage retail, service and residential uses within existing buildings and new development.</p> <p>Maintain and expand governmental and public services within the Downtown.</p> <p>Facilitate the preservation and rehabilitation of historic commercial buildings while fostering good infill development design.</p> <p>Consider the reuse of older homes adjacent to the Downtown core for small-scaled retail uses.</p> <p>Promote a safe and comfortable pedestrian environment through transportation and urban design enhancements.</p>
Establish thriving neighborhood retail and destination commercial centers along Amity Street and Metcalf Road.	<p>Direct neighborhood-scale commercial development along Amity Street and Metcalf Road, and larger-scaled destination commercial uses near the Highway 68/69 interchange.</p> <p>Promote high-quality development design, signage, and reduce curb-cuts and driveway aprons where feasible and warranted.</p> <p>Limit expansion of commercial uses along corridors until market demand strengthens.</p>
Strengthen Louisburg's stable, vibrant neighborhoods that provide varied housing and living opportunities.	<p>Maintain, rehabilitate, and preserve the older, traditional housing stock adjacent to the Downtown.</p> <p>Facilitate build-out of unfinished residential subdivisions.</p> <p>Encourage connectivity between developing and existing neighborhoods.</p> <p>Promote duplex and multi-family housing developments near the Downtown and adjacent to schools, parks and existing residential neighborhoods.</p> <p>Support new, high-quality housing development design.</p>
Strengthen and expand Louisburg's enhanced and well-maintained parks and open space system.	<p>Encourage developers to pursue high-quality public space improvements, parks, open space, and trails within new residential developments.</p> <p>Encourage park development in underserved areas of the City, specifically the west-northwest areas, to provide walkable access to outdoor recreation opportunities.</p>
Ensure Louisburg has a well-connected, maintained and comprehensive transportation system.	<p>Promote safe, comfortable pedestrian and vehicular connectivity between different districts, neighborhoods, community destinations, and Downtown.</p> <p>Maintain all roads and rights-of-way to promote efficient transportation and circulation within the community.</p> <p>Consider future impacts of driverless car technology</p>
Ensure Louisburg has an orderly approach to maintaining its city form and addressing growth management.	<p>Ensure capital improvement planning and expenditures are consistent with the land use and development goals presented in the Future Land Use Strategy.</p> <p>Collaborate with other local, county and regional agencies regarding infrastructure, capital improvements and growth management issues.</p>
Maintain a high quality-of-life with quality community services, amenities and year-round activities.	<p>Support expansion and improvement of community services.</p> <p>Promote community identity through gateway, wayfinding and other urban design enhancements.</p> <p>Involve other partners and entities in programs and events that build community relationships and social equity.</p>



This Comprehensive Plan document - a document representing a clear, compelling vision and framework for Louisburg’s long-term growth, development and quality of life - was created through an energetic, collaborative planning process undertaken between the City of Louisburg, the Comprehensive Plan Steering Committee, the Planning Commission, and the residents and stakeholders of Louisburg.



The collaborative process included a community speak-out session, a workshop with Louisburg youth, a series of stakeholder interviews, and meetings with the Comprehensive Plan Steering Committee - a process that helped garner a broad understanding of planning issues that are of critical concern to the people who live and work in the community.

A starting point and foundation element for the Louisburg Comprehensive Plan is the Vision Statement – a statement that summarizes the community’s consensus and aspirations for Louisburg’s “bright future.” From the Vision Statement, a series of planning goals, policies and strategies have been created to guide community decision-making in maintaining and enhancing Louisburg’s vitality and quality-of-life.

All goals and strategies build on the community’s existing assets – its neighborhoods; its Downtown district and commercial areas; its schools, parks and amenities; and, its vibrant network of civic and religious institutions – while exploring ways in which to grow and develop without diminishing its small-town character and tightly-knit social networks – qualities that make Louisburg distinctive among all other communities within the greater Kansas City region.

Succeeding sections of this chapter – ***Community Vision and Future Land Use*** – provide an overview of the Comprehensive Plan’s recommended land use strategy for Louisburg’s future development and revitalization. The land use strategy is the Comprehensive Plan’s central element as it provides the blueprint for how and where Louisburg will grow over the next decade, ensuring that a high-quality built environment and a balance of land uses are achieved. A balanced and appropriate mix of land uses, from residential, commercial, parks and open space, and industrial, can help promote and enhance Louisburg as an attractive place to invest in as well as live, work, shop, and play.

LOUISBURG VISION STATEMENT

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Louisburg’s Vision Statement captures the community’s aspirations, sentiments and values for its long-term growth and quality of life; it also defines what aspects of the community should be preserved and maintained as key building blocks for future prosperity and growth, and what should be changed to promote quality living, sustainability and resilience in the face of ever-changing economic conditions. While the Vision Statement defines the community’s aspirations, it also serves as the benchmark for measuring the progress of implementing the Bright Future Plan’s goals, strategies and initiatives over time.

In 2027, Louisburg is the quintessential American small town known in the Kansas City Region for the vibrancy and character of its Downtown and residential neighborhoods, the quality of its public services and amenities, and the authenticity and friendliness of the people who live and work in the community. Over the years, the City, along with its committed residents and stakeholders, have planned for and invested in its assets and pursued opportunities for quality, measured growth and revitalization that strengthens the community’s economic base, sense of place and overall quality of life. Louisburg always strives to plan and improve the community with the active and vigorous involvement and engagement of its stakeholders and citizens.

While considering the opportunities and constraints for Louisburg’s growth and development, the community has achieved the following:

A vital Downtown and desirable shopping areas.

Downtown Louisburg has become a center for retail, service, and business activities, and, with its attractive streetscape, event spaces, and pedestrian environment, an appealing place for social interactions and community gatherings throughout the year. Downtown buildings have been rehabilitated and preserved, while new development – appropriate to Downtown’s sense of scale and character – provides additional space for new businesses and housing for those who want to live in or near the Downtown. A new Public Library, offering expanded services to Louisburg residents, along with City Hall, serve as key institutional anchors to an ever-vital and growing Downtown.

While Downtown provides a mix of commercial, residential, and institutional uses, the destination shopping areas along Amity Street and Metcalf Road serve both customers from surrounding communities and the daily shopping needs of local residents. Gateway signage, landscaping, wayfinding, and better building and site design enhance the appearance of these important commercial corridors. Bicycle routes, sidewalks, crosswalks, and other pedestrian amenities connect Downtown and other shopping areas to adjacent neighborhoods, schools, and parks.



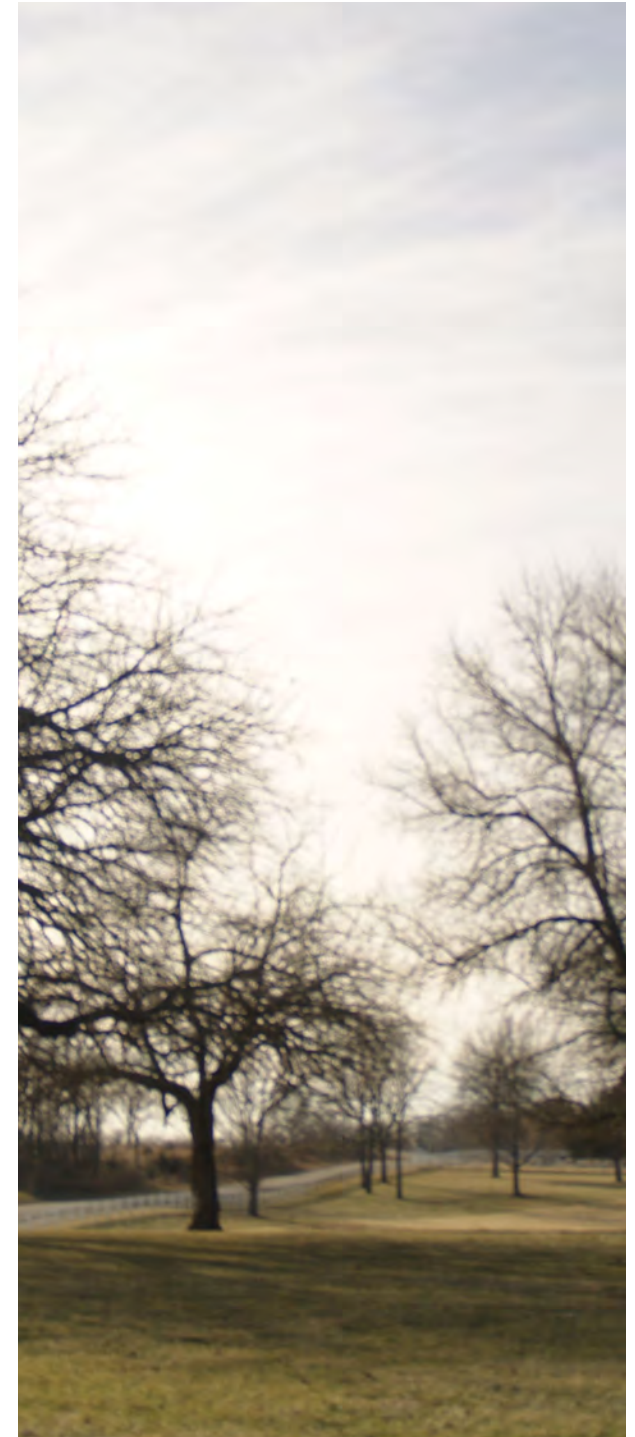
Livable, quality neighborhoods.

Louisburg boasts a diversity of neighborhood choices – within established, traditional neighborhoods near the Downtown, and in growing sectors of Louisburg with easy access to local schools, open spaces and recreational amenities. Louisburg’s traditional neighborhoods will be maintained, preserved and enhanced through a combination of public and private investments. Property maintenance programs and incentives serve to facilitate private improvements while ongoing City capital investments improve the sidewalk network, rights-of-way, signage, landscaping and visual identity of the traditional neighborhoods.

New residential growth occurs in a measured, anticipated pace in suitable locations that incorporate visually appealing residential design, sidewalks, parks, trails, and other natural features. Existing neighborhoods and new housing products accommodate a variety of housing needs – from new families and households to senior citizens – in housing designs that fit in and complement Louisburg’s small-town character and landscape.

An enviable quality of life.

Louisburg will be known in the Kansas City metropolitan region for its good schools, accessible and well-maintained parks, local employment centers, active civic and religious institutions, community events and festivals, bike and pedestrian trails, preserved landmarks, and responsive community services. Schools continue to be centers for academic excellence; city parks, including Lewis-Young Park, have enhanced amenities for a variety of sports and recreation activities; the Senior Center expands its services to help Louisburg’s elders remain active and connected with others; the Public Library builds a new Downtown facility with programs that enlighten and spark the mind; and, local churches and religious institutions work together on initiatives that address the social and spiritual needs of the people of Louisburg. Louisburg continues to provide and deliver a high level of community services to its residents as a way to support and enhance the local quality of life, public health, community engagement, and social cohesion and interaction.



PLANNING PRINCIPLES



Along with the Vision Statement, a series of guiding planning principles have been prepared to carry out the Bright Future Plan Vision and achieve its successful implementation. The principles reflect core community values, as determined through the stakeholder engagement process, and the three main planning themes of Livable Community, Vibrant Community, and Connected Community.



Planning principles are also the “essential” and “fundamental” considerations when implementing aspects of this Comprehensive Plan, including its land use strategies. In view of future growth trends, Louisburg’s short and long-term development program should align with this Comprehensive Plan’s goals and policies.

Livable Community

A “livable community” is defined by its built environment, housing and neighborhoods, community services, natural areas and parks, churches and schools, and cultural and recreation opportunities. A livable community equates to a high quality of life – a high quality of life attracts new investment, contributes to community cohesion, and builds local pride of place. Livable community principles are outlined on the following page.

Vibrant Community

A “vibrant community” is defined by a bustling Downtown district, with businesses, services and activities that draw Louisburg residents and visitors from around the Kansas City area, as well as other successful and thriving neighborhood and destination shopping districts. A community’s business and economic

base should be diversified enough to be resilient to any economic changes at the regional, state and national levels. Vibrant community principles are outlined on the following page.

Connected Community

A “connected” community is one that has adequate transportation and mobility options, including a well-maintained roadway network, and bicycle and pedestrian facilities. A connected community is also one that provides opportunities for engagement in civic affairs and collaboration among different groups in community-initiated programs and activities. Connected community principles are outlined on the following page.



LIVABLE

Create community and neighborhood identity through enhancements to buildings, public spaces and other aspects of Louisburg’s physical environment.

Provide choices and diversity in housing, shopping and employment opportunities to accommodate residents in all stages of their lives.

Maintain, improve and protect Louisburg’s parks, open spaces and natural areas as an interconnected system of green infrastructure, outdoor recreational spaces and distinctive landscapes.

Encourage the preservation and conservation of important buildings, sites, landmarks, neighborhoods and landscapes that contribute to Louisburg’s sense of place.

Improve community health by providing access to healthy foods, quality recreational programs, and other activities and initiatives that promote active living.



VIBRANT

Offer multiple options in which people can walk, bicycle and travel in Louisburg.

Ensure all transportation systems – roadways, sidewalks, bike trails – **provide safe and secure travel environments.**

Enhance existing facilities and consider the development of new amenities and venues that increase social participation and citizen engagement activities.

Consider new services and outreach initiatives that facilitate interaction and participation among groups with special social and economic needs.

Build social capital by supporting the work of local non-profit organizations, civic groups and new associations that offer volunteer and community engagement opportunities.



CONNECTED

Enhance the appearance, function and quality of the traditional Downtown and other commercial areas to attract local consumers and regional visitors.

Achieve a diversified land use base with a proper balance of commercial, industrial and job-creating land uses.

Improve the local business climate by supporting entrepreneurial activity and small business development.

Ensure planning and land development regulations facilitate Downtown revitalization and the quality commercial and industrial growth.

Support and participate in regional economic development initiatives that may have important impacts on the community’s long-term resiliency and sustainability.

FUTURE LAND USE STRATEGY & MATRIX

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Achieving the community’s short and long-term planning, growth and revitalization goals will require a well-considered approach to community land use - an approach that builds a balance between residential and commercial growth while encouraging new land uses that provide opportunities for local job growth and economic development.

















A balance between such land uses in Louisburg is needed to ensure a stable and growing tax base that promotes economic diversity and community resiliency to changes in local and regional economic conditions. Currently, a heavy share of the tax burden is borne in Louisburg’s residential sector, potentially making Louisburg less attractive to future homebuyers if local property taxes continue to climb over time.

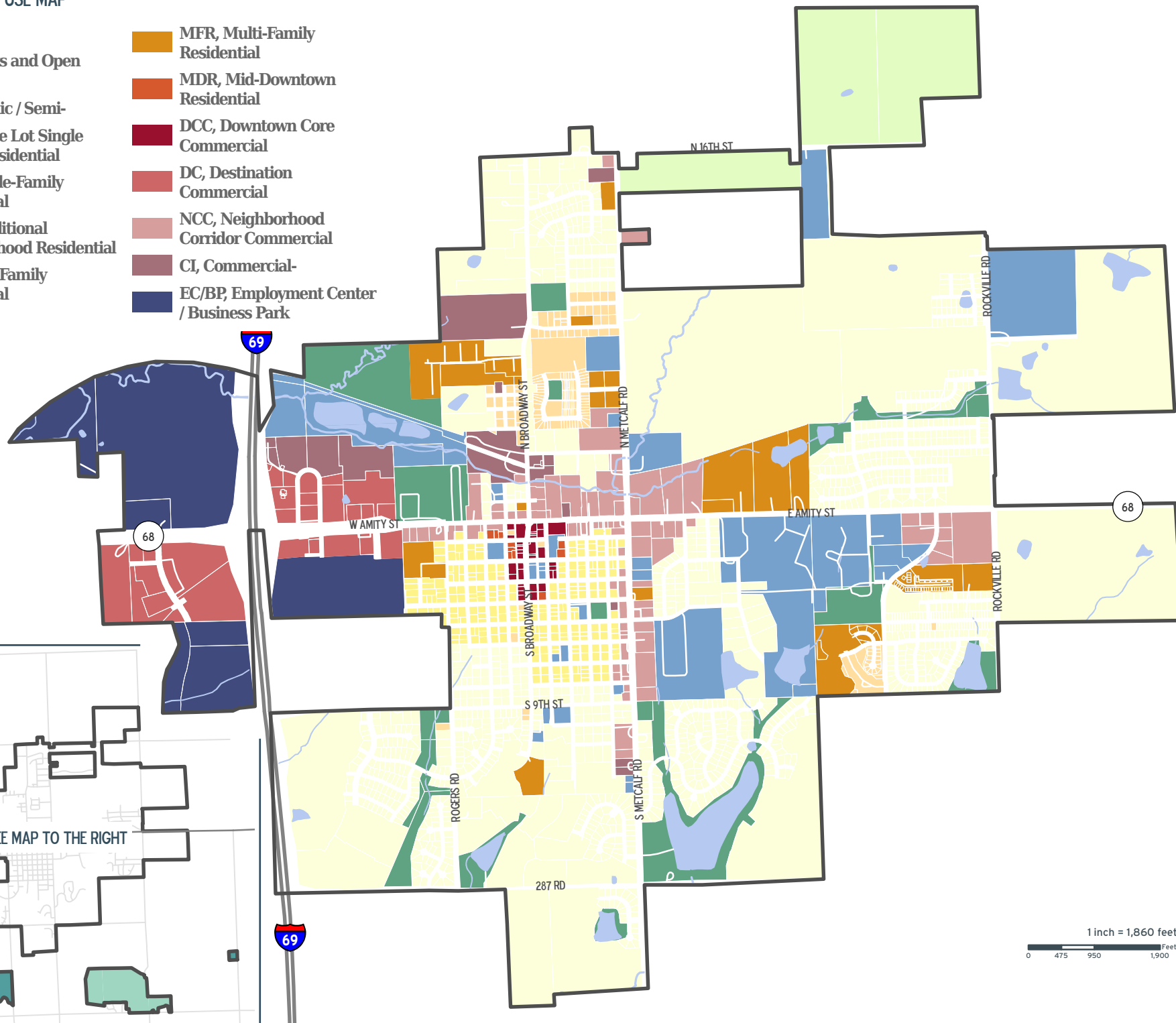
The Future Land Use Map (on following pages) depicts a strategic, recommended pattern of land uses in the City and the form, types and intensities of different land uses occurring in a particular area. The following definitions describe each land use category proposed in the Future Land Use Map. While the land use map should guide future land use and development and zoning decisions, it is also meant to be adjusted and changed when circumstances warrant a change in planning direction in any given area of the City. However, any changes to the Future Land Use Map should also be consistent with the larger community vision presented in this Bright Future Plan. It should also be noted that the land use strategy does not eliminate existing uses on existing properties where a change in land use direction is recommended in this Plan. Specific land use objectives are described in later sections of this Comprehensive Plan.

Land Use Matrix

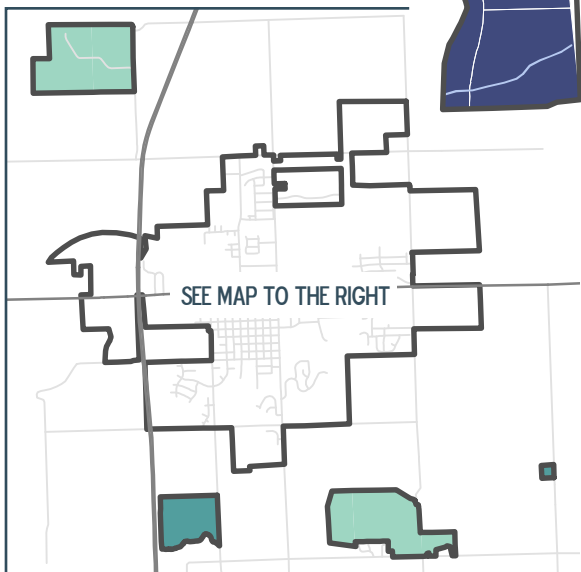
The Land Use Matrix on the following pages summarizes the Future Land Use Strategy and how each land use can be applied given current conditions. Flexibility in implementing the Future Land Use Strategy is allowed while being consistent in achieving the overall planning goals presented in this Comprehensive Plan.

PROPOSED LAND USE MAP

-  City
-  POS, Parks and Open Space
-  P/SP, Public / Semi-
-  LLR, Large Lot Single Family Residential
-  SFR, Single-Family Residential
-  TNR, Traditional Neighborhood Residential
-  TFR, Two-Family Residential
-  MFR, Multi-Family Residential
-  MDR, Mid-Downtown Residential
-  DCC, Downtown Core Commercial
-  DC, Destination Commercial
-  NCC, Neighborhood Corridor Commercial
-  CI, Commercial-
-  EC/BP, Employment Center / Business Park



CITY-WIDE MAP





COMMERCIAL LAND USES

Permitted Uses

Use Features and Characteristics

DOWNTOWN CORE COMMERCIAL (DCC)

Mix of commercial uses, such as small retail, restaurants, small service businesses.

Upper-story office and residential units.

Institutional-government uses, including City Hall, Public Library and other similar use and facility types.

Rehabilitation of historic commercial buildings; storefront, façade improvements, and adaptive use is of high importance.

New single or mixed development on infill parcels; new development compatible with Downtown’s design character.

Pedestrian-oriented development with zero-lot line along Broadway Street.

Quality streetscape and urban design in the public right-of-way and pedestrian linkages to adjacent neighborhoods and Amity Street.

Small retail uses accommodated in traditional housing along 3rd and Peoria Streets.

NEIGHBORHOOD COMMERCIAL CORRIDOR (NCC)

A variety of small-scaled auto-oriented commercial uses and retail centers, independent businesses, offices and restaurants located along Amity Street and Metcalf Road.

Setbacks along Amity Street and Metcalf Road with parking located to side or rear of building; shared access between developments encouraged.

High quality site, building design, and signage.

Landscape buffering and treatments with development located directly adjacent to residential and institutional land uses.

Lighting, landscaping, wayfinding, crosswalks, and other pedestrian improvements implemented within the corridor ROW where feasible and allowed.

DESTINATION COMMERCIAL (DC)

A variety of medium to large-scale auto-oriented commercial uses, such as big box stores, hotels, restaurants, car dealerships and retail chains located adjacent to Amity Street / K-68 Highway and U.S. 69 intersection.

Large setbacks maintained along Amity Street.

High quality site and building design, landscaping, signage, and shared access between developments encouraged.

Gateway, lighting, and landscaping improvements with landscaping recommended between parking lots, rights-of-way, and lower intensity land uses.

Pedestrian linkages to the traditional neighborhoods east along Amity Street encouraged.

COMMERCIAL INDUSTRIAL (CI)

Small to large-scale developments accommodating commercial uses such as auto body and repair shops, contracting offices, feed and supply stores, and storage yards.

Located preferably along secondary and collector streets rather than arterials.

High quality site and building design and signage.

Setbacks recommended from roadway.

Adequate screening and placement of outdoor storage areas.

RESIDENTIAL LAND USES

Permitted Uses

Use Features and Characteristics

TRADITIONAL NEIGHBORHOOD RESIDENTIAL (TNR)

Detached single-family residential.

Maintenance, rehabilitation and preservation of existing single-family housing stock.

Infill single-family permitted on large lots compatible with neighborhood's existing design, scale, and character.

Improved pedestrian connections to Downtown with sidewalk and crosswalk enhancements.

Stormwater management and other capital improvements.



MID-DOWNTOWN RESIDENTIAL (MDR)

Duplex and townhome residential.

Vertical duplex and townhome residential types up to two stories.

Small lot detached single-family residential encouraged.

Residential design compatible with adjacent traditional single-family neighborhoods.

Enhanced connectivity to Downtown via sidewalks, street lights and other urban design enhancements.



LARGE LOT SINGLE FAMILY RESIDENTIAL (LLR)

Restricted to single-family on large lots (3-5 acres).

Densities may range from 1 units per 3 to 5 acres.

Planned unit developments and conservation subdivisions of clustered housing.

Development clustering and the protection of environmental and historic resources and viewsheds encouraged.

Developments provided with full municipal and infrastructure services.

Should not be located near employment centers or commercial areas.

Connectivity with adjacent residential areas encouraged.



SINGLE FAMILY RESIDENTIAL (SFR)

Detached single family residential.

Single-family housing up to two stories with attached or detached garages.

Architectural and housing type diversity encouraged.

Small-lot detached single-family permitted where desired and feasible.

Incorporation of sidewalks, trails, open space and natural features.





RESIDENTIAL LAND USES, CONTINUED

Permitted Uses

Use Features and Characteristics

TWO-FAMILY RESIDENTIAL (TFR)

Attached two-family residential.

Two-family townhomes up to two stories and horizontal duplexes.
 Architectural diversity encouraged.
 Shared driveway aprons encouraged.
 Street and pedestrian connectivity with adjacent residential and commercial areas.

MULTI-FAMILY RESIDENTIAL (MFR)

Fourplexes, town/rowhomes and apartments.

Fourplexes up to two stories; apartments up to three stories.
 Recommended in areas with existing multi-family developments or in areas adjacent to Single- and Two-Family neighborhoods.
 Street and sidewalk connectivity.
 Incorporation of open space and natural areas.

ALL OTHER LAND USES

Permitted Uses

Use Features and Characteristics

EMPLOYMENT CENTER/BUSINESS PARK (EC/BP)

Small to medium-sized employment-generating uses.

Warehousing, research, light assembly and low-impact manufacturing uses that are wholly contained in a building and do not generate noticeable external effects.

Restricted to peripheral areas along Amity Street (Kansas Highway 68 and U.S. Highway 69).

Uses screened from land uses of lower intensity.

Development should be of high quality building and site design.

PUBLIC/SEMI-PUBLIC (P/SP)

Government buildings, public service facilities, schools and educational institutions, non-profit-owned land uses and religious institutions.

Public/Semi-Public buildings should be of high quality building and site design.

Sidewalks, bike paths and trails should be considered to connect Public/Semi-Public uses to other neighborhoods and districts.

Shared parks and open spaces added as part of governmental/school building campus settings.

Permitted by condition in most other land use classifications; conditions may focus on traffic, parking, design and other operating characteristics.

PARKS AND OPEN SPACE (P/OS)

Parks and open spaces for active and passive recreational uses.

Park facilities are maintained and upgraded on an ongoing basis according to a parks master plan.

Parks and open spaces may be established as part of future residential developments.

Connected by sidewalks and trails to adjacent residential neighborhoods and commercial districts.

Permitted in residential neighborhoods.

AGRICULTURAL LAND (AL)

Farming and other agricultural-related uses.

Farming and other related agricultural uses.

Agricultural use impacts minimized to adjacent land uses of higher intensity and environmentally-sensitive areas.

Land will remain in agriculture or open space use during this Comprehensive Plan's time horizon.



COMMERCIAL LAND USES

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The Future Land Use Plan classifies commercial into four categories: Downtown Core Commercial, Neighborhood Corridor Commercial, Destination Commercial and Commercial-Industrial. These land use categories are summarized in the following pages.



Downtown Core Commercial (DCC)

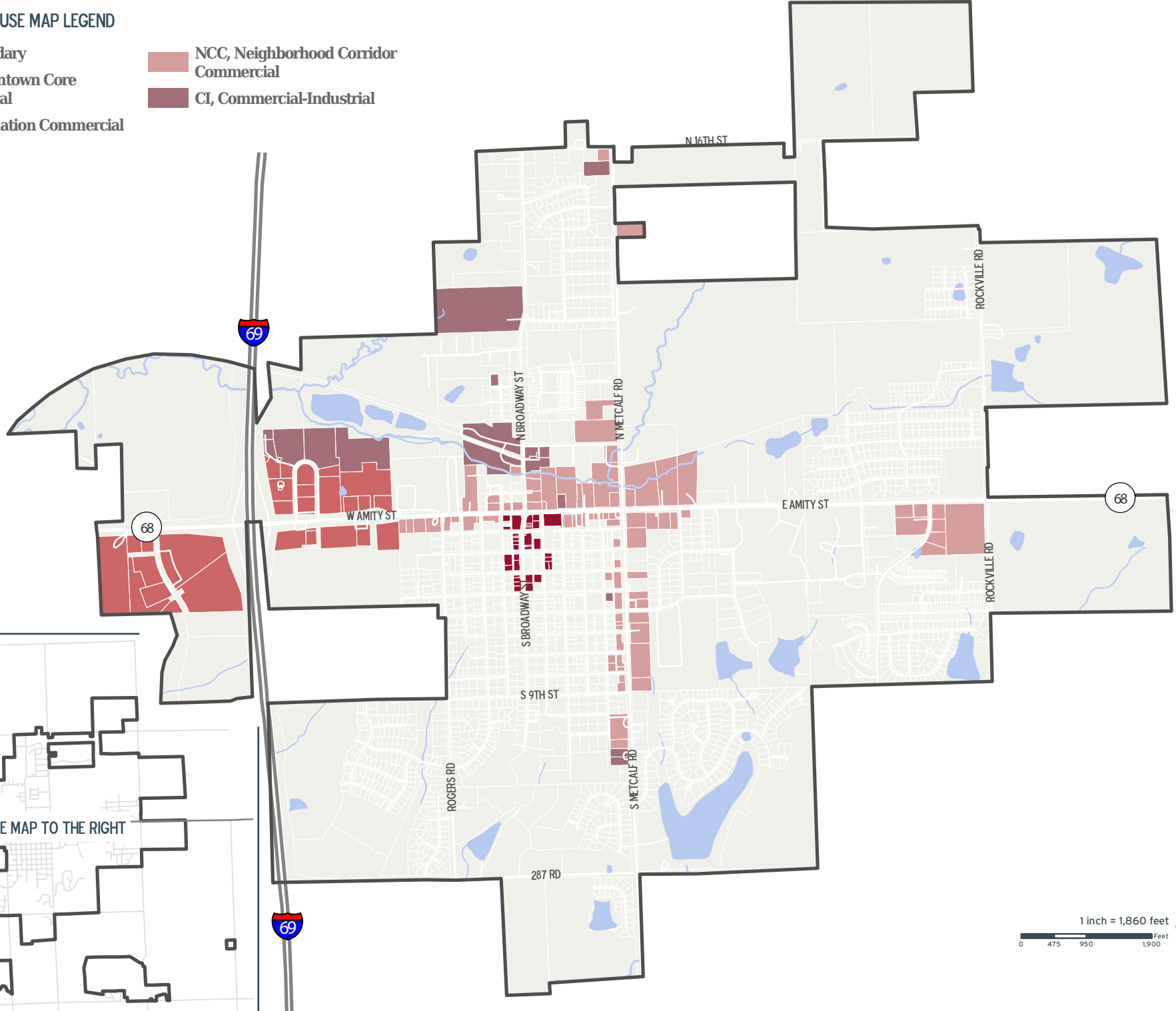
Roughly bounded by Doyle Street on the west, Amity Street (Highway 68) on the north, Elm Street on the east and South 4th Street on the south, the Downtown Core Commercial is intended to encourage a mix of commercial, residential and institutional uses in both existing buildings and new construction with the main goal of revitalizing the Downtown as a mixed-use center of activity. Existing historic commercial buildings along Broadway Street should be a central focus of rehabilitation and reuse with retail and service uses occupying first floors and offices and residential occupying upper floors where opportunities exist.

Some limited forms of light assembly uses associated with a retail use may also be permitted and encouraged. As suggested in the *2014 Downtown Louisburg Revitalization Plan*, smaller, “boutique” retail uses, such as restaurants and specialty stores, could be encouraged in converted traditional homes along South 3rd and Peoria Streets as a way to accommodate an expansion of Downtown’s commercial floor area.

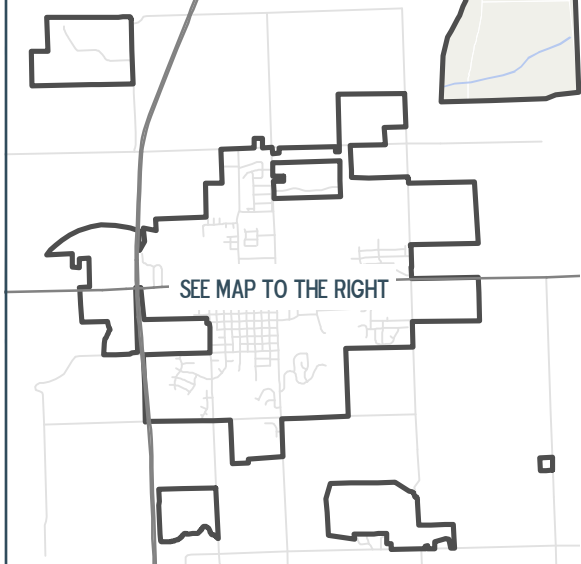
Downtown will remain an institutional-governmental center with its City Hall complex along with a potential new or expanded Library facility and public plaza between Doyle, Mulberry, South 1st, and South 2nd Streets. New mixed-use development would also be encouraged along Broadway Street and other vacant and underutilized parcels. Allowing a mix of uses in the Downtown core will help facilitate increased utilization of buildings and spaces and help build value in Downtown real estate and businesses.

PROPOSED LAND USE MAP LEGEND

-  City Boundary
-  DCC, Downtown Core Commercial
-  DC, Destination Commercial
-  NCC, Neighborhood Corridor Commercial
-  CI, Commercial-Industrial



CITY-WIDE MAP





Neighborhood Corridor Commercial (NCC)

Smaller-scaled commercial establishments located principally along Amity Street and Metcalf Road will provide opportunities for a diversity of commercial activities, including opportunities such as small-scaled retail establishments, restaurants, salons, clinics and doctor offices, private athletic clubs, banks, and groceries. Neighborhood corridor developments are usually characterized as stand-alone, single story single use pad sites or within auto-oriented strip centers with parking located to the front of the development and access to the roadway through curb cuts and driveway aprons. This Comprehensive Plan does not propose any expansion of Neighborhood Corridor Commercial land uses within Louisburg; rather, planning goals and policies for existing areas include urban design and streetscape improvements, curb cut consolidation, site enhancements, and business development and recruitment.

Destination Commercial (DC)

Portions of Amity Street between U.S. 69 and land just west of Rogers Road are classified as Destination Commercial given the concentration of large-scale destination-type retail and strip development centers. Destination commercial uses – grocery and general merchandise stores in particular – are those that serve both local residents and customers from surrounding communities; other destination commercial use types include hotels, banks, financial institutions, auto dealerships, restaurants, and office complexes. This land use classification also reflects its predominate design characteristics, mainly single-story in height with deep setbacks from the roadway and large parking lots located to the front or side of the development. The Destination Commercial node east of U.S. 69 is largely built out on the north side apart from available pad development sites along Amity Street south of the Orscheln Farm and Home strip center and between First National Bank and McDonald’s. No new Destination Commercial areas are proposed in this Comprehensive Plan; however, hotels and offices could conceivably be developed in the U.S. 69 Destination Commercial zone.

Commercial-Industrial (CI)

Commercial-Industrial land uses are uses with light-industrial, manufacturing and outdoor storage activities and needs associated with commercial, contracting and office uses – the purpose of the land use classification is to support such uses that may serve as significant employers in the community, as a buffer between land uses of a lower intensity and general industrial uses, or to accommodate commercial-industrial activities that may not be suitable in certain locations. Commercial-Industrial uses are located on land just north of the Destination Commercial node at Amity Street east of Highway 69 and in land parcels along Amity Street east of Metcalf Road. No new Commercial- Industrial areas are proposed in this Comprehensive Plan.



RESIDENTIAL LAND USES

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The Future Land Use Plan classifies residential land into six categories: Large Lot Single-Family Residential, Traditional Neighborhood Residential, Mid-Downtown Residential, Single Family Residential, Two-Family Residential and Multi-Family Residential. These land use categories are summarized in the following pages.



Large Lot Single-Family Residential (LLR)

Large Lot Residential neighborhoods accommodate low-density detached single-family housing on large lots between three (3) to five (5) acres in size. These neighborhoods would be located in Louisburg's northeastern quadrant north of West 271st Street and east of Rockville Road, and for land south of 271st Street and east of Metcalf Road. While conventional detached housing development and subdivision layout would be permitted, the clustering of homes on smaller lots to preserve open space, cultural resources, scenic views and other environmental features would be encouraged.

Traditional Neighborhood Residential (TNR)








Traditional Neighborhood Residential areas comprise the older neighborhood blocks to the east, west and south of Downtown Louisburg. These blocks are characterized by historic homes dating from the time of Louisburg's early settlement to Post World War II housing types, including ranch and split-level homes. The area is also defined

by the square and rectilinear block pattern emanating from the Downtown. This land use classification seeks to maintain and preserve the neighborhood's traditional single-family character by encouraging housing maintenance and rehabilitation along with infill housing sensitive to the design and scale of the housing stock. Capital improvements, including new sidewalks, stormwater management and other right-of-way enhancements should be considered where needed and implemented.

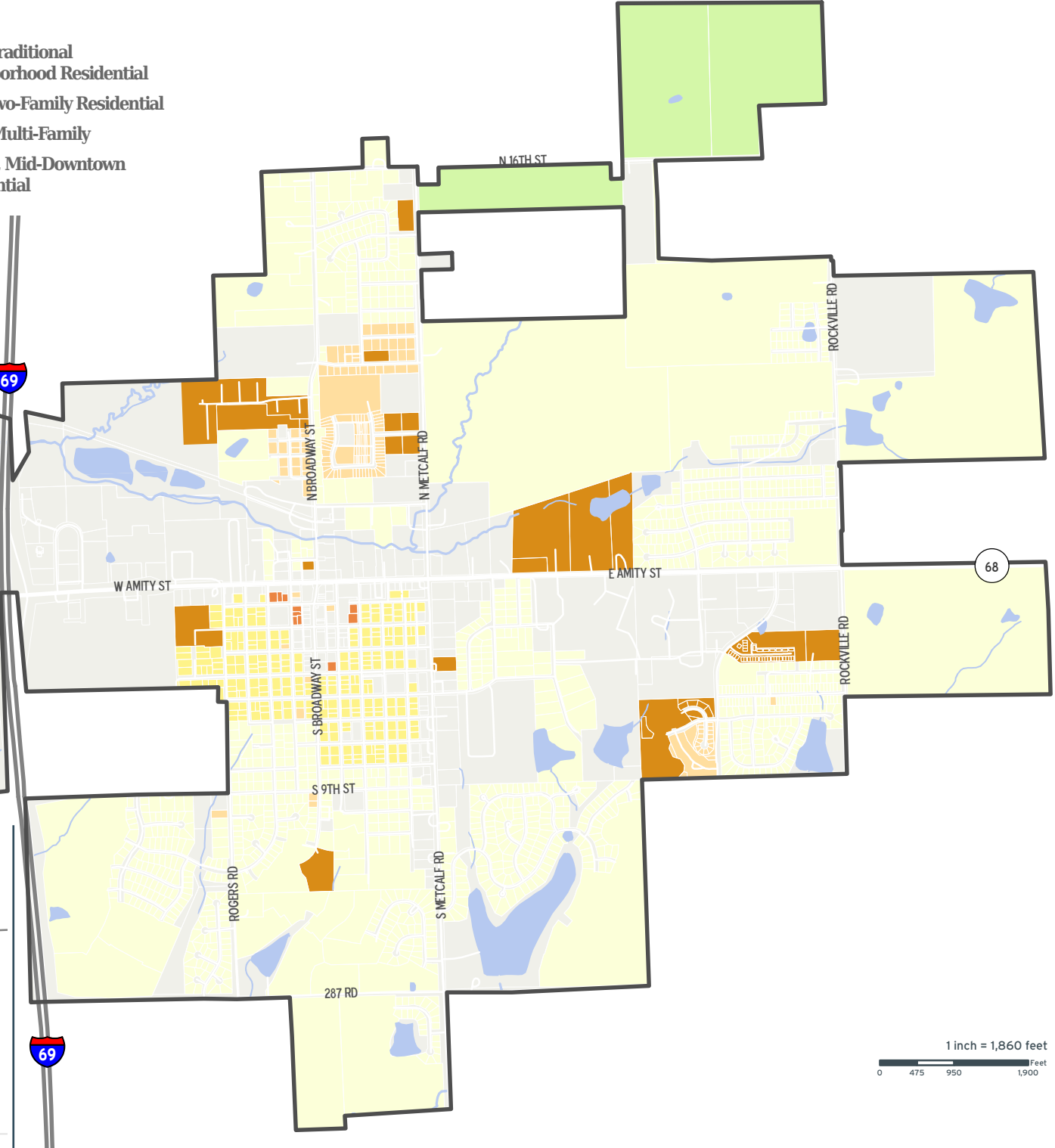
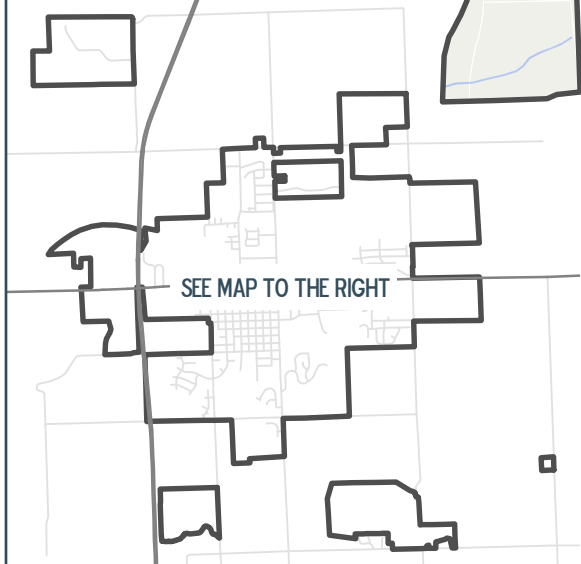
Mid-Downtown Residential (MDR)

Mid-Downtown Residential neighborhoods comprise the blocks and half-blocks just outside the core Downtown-Broadway Street commercial core, including the blocks along Mulberry Street between Amity and 2nd Streets and at Elm Street between 1st and 2nd Streets. The purpose of the Mid-Downtown Residential land use classification is to encourage duplex and townhome type developments, potentially providing housing opportunities for seniors and young households adjacent to the Broadway Street commercial area. Such housing types should be designed to be compatible and similar in scale to the traditional single-family housing found to the south, east and west of the Downtown

PROPOSED LAND USE MAP LEGEND

-  City
-  LLR, Large Lot Single-Family Residential
-  SFR, Single-Family
-  TNR, Traditional Neighborhood Residential
-  TFR, Two-Family Residential
-  MFR, Multi-Family
-  MDRN, Mid-Downtown Residential

CITY-WIDE MAP





core. This land use type was first proposed in the 2014 Downtown Revitalization Plan.

Single-Family Residential (SFR)

Single-Family Residential neighborhoods are in areas of Louisburg that have largely been developed over the last 20 years and located to the northeast, southeast and southwest of the Traditional Neighborhood Residential. Future Single-Family Residential neighborhoods are proposed for recently annexed areas south of West 271st Street, east and west of Rockville Road near Amity Street, and in locations to the south along Metcalf Road. Existing neighborhoods feature conventional subdivision layouts with long blocks, curvilinear streets and neo-traditional housing designs with attached garages that face the street. It is the intent of this land use classification to create well-designed, walkable neighborhoods that incorporate open space and appropriate linkages to surrounding neighborhoods and districts. Neo-traditional subdivision designs with shorter blocks, gridded streets and detached garages should be encouraged.

Two-Family Residential (TFR)

Like the Single-Family Residential neighborhoods, Two-Family Residential neighborhoods are in areas and districts that have been recently developed – mainly near Harvest Drive and South 5th Street in Louisburg’s east side near the High School/Aquatic Center complex. Two-Family Residential is characterized by townhomes, rowhouses and duplexes where single-family units share the same partywall. Two-Family Residential neighborhoods are proposed to remain and expand along Harvest Drive to the north.

Multi-Family Residential (MFR)

Multi-Family Residential is defined as more than two housing units per lot consisting of stand-alone fourplexes, apartments, condominiums and senior housing developments. Existing multi-family areas in Louisburg are found along Harvest and Danford Drives primarily. Multi-Family Residential neighborhoods are proposed to remain and expand along Harvest and Danford Drives where development opportunity exists; Multi-Family is also suggested along portions of Amity Street north and west of the High School complex and east of an existing single-family subdivision, principally to provide additional housing opportunities for singles, young households and seniors.



EMPLOYMENT CENTER/BUSINESS PARK (EC/BP)

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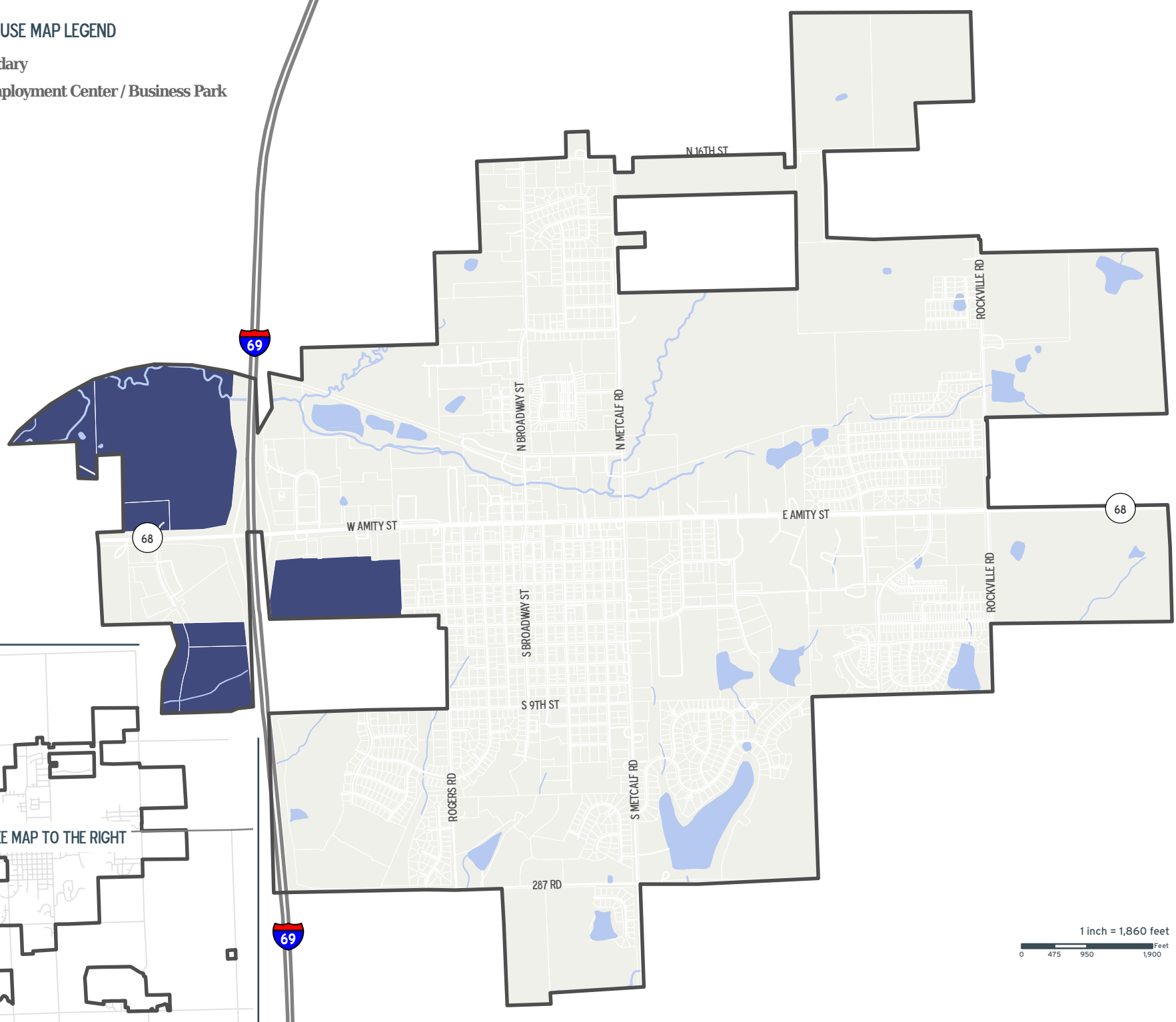
The Future Land Use Strategy recommends the establishment of an Employment Center/ Business Park land use classification to encourage a broad range of employment-generating enterprises - small light-assembly and fabrication businesses, warehousing, research and development, technology centers, back-office operations, and other related industry types - enterprises that have minimal or no environmental impact.

Such businesses will be located along the Amity Street arterial near the U.S. Highway 69 interchange so as to facilitate easy access and service by truck transportation. Development within these areas should be designed in a cohesive modern business park setting with wide streets, ample landscaping treatments and gateway signage.

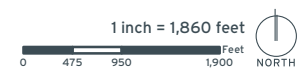
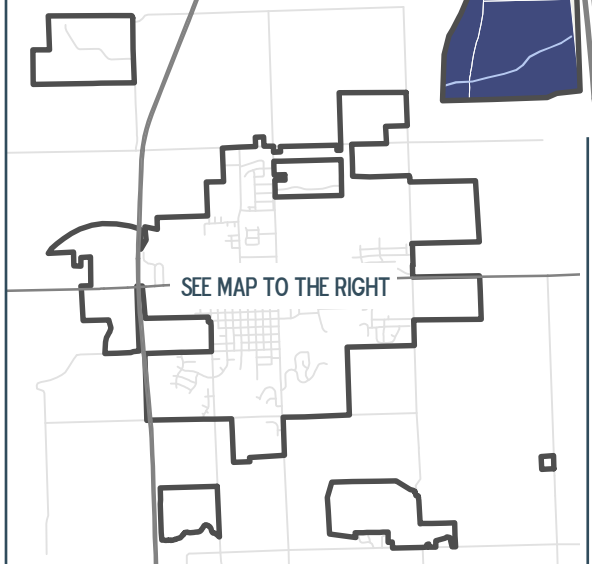


PROPOSED LAND USE MAP LEGEND

-  City Boundary
-  EC/BP, Employment Center / Business Park



CITY-WIDE MAP



PUBLIC/SEMI-PUBLIC (P/SP)

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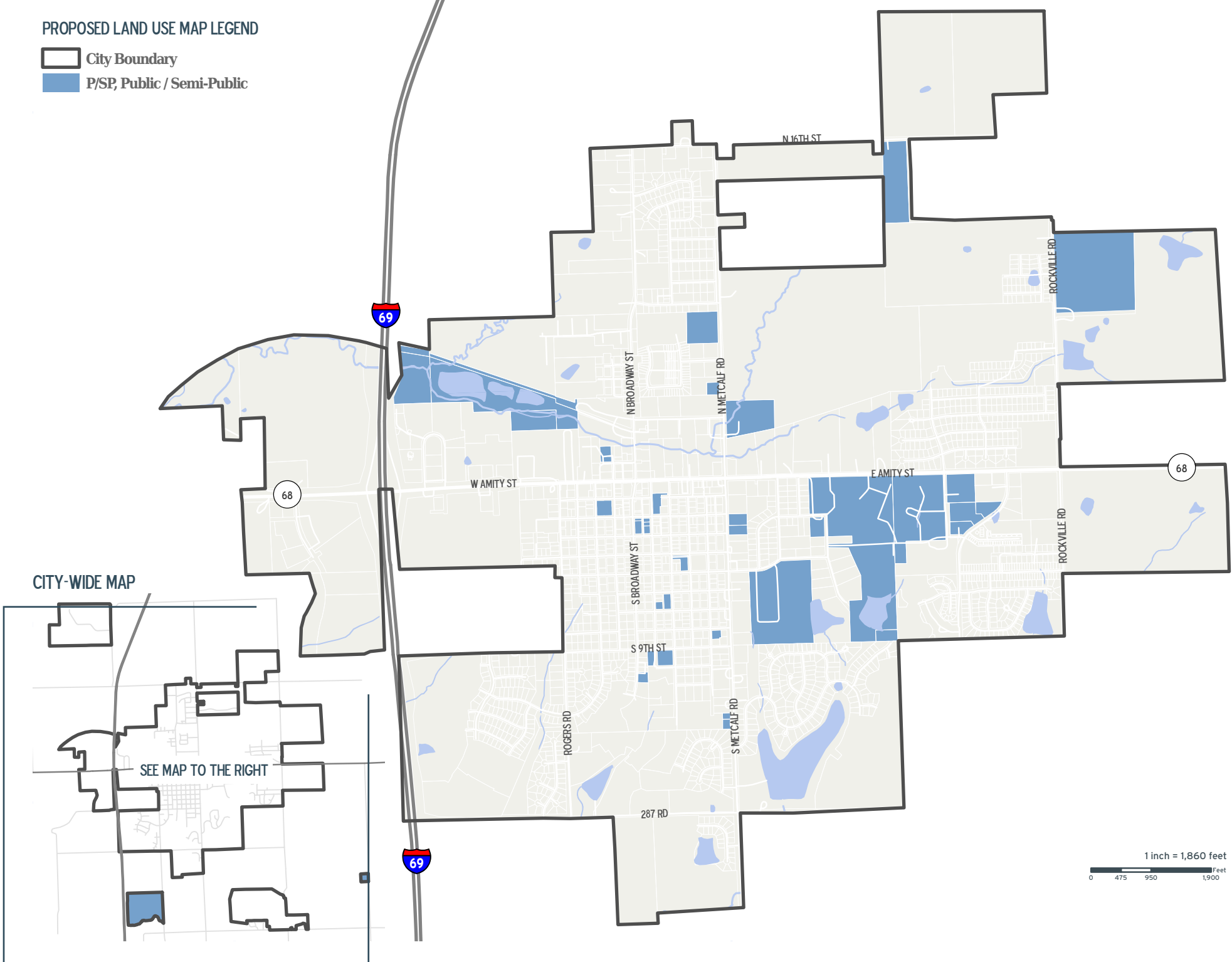
Louisburg municipal facilities, including its City Hall, Fire and Police Stations, local schools, and the Public Library, as well as other government-owned buildings or parcels, such as the U.S. Post Office are classified as Public/Semi-Public.

The City's sanitary facilities and lagoon areas are also classified as Public/Semi-Public, along with utilities, religious buildings and other non-private land uses. Except for the newly-planned sanitary facilities, it is anticipated that existing Public/Semi-Public uses will remain largely as they currently exist. The Public Library has been planning for a new facility over the last several years; this Comprehensive Plan is proposing the Library remain within the Downtown Core Commercial land use zone.

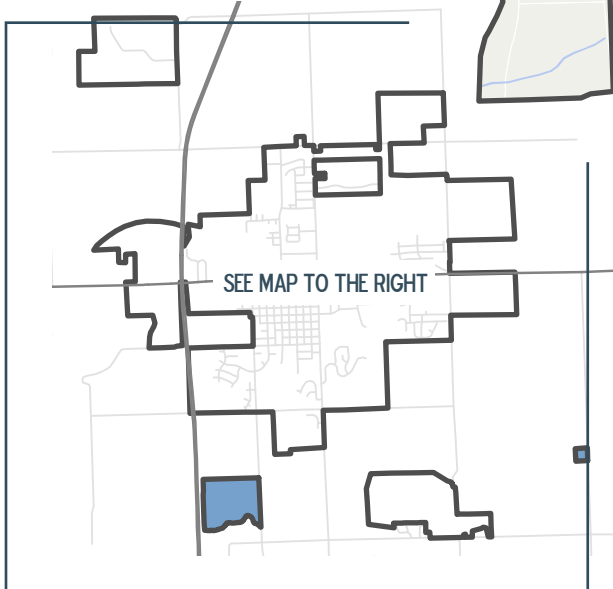


PROPOSED LAND USE MAP LEGEND

- City Boundary
- P/SP, Public / Semi-Public



CITY-WIDE MAP



PARKS AND OPEN SPACE (P/OS)

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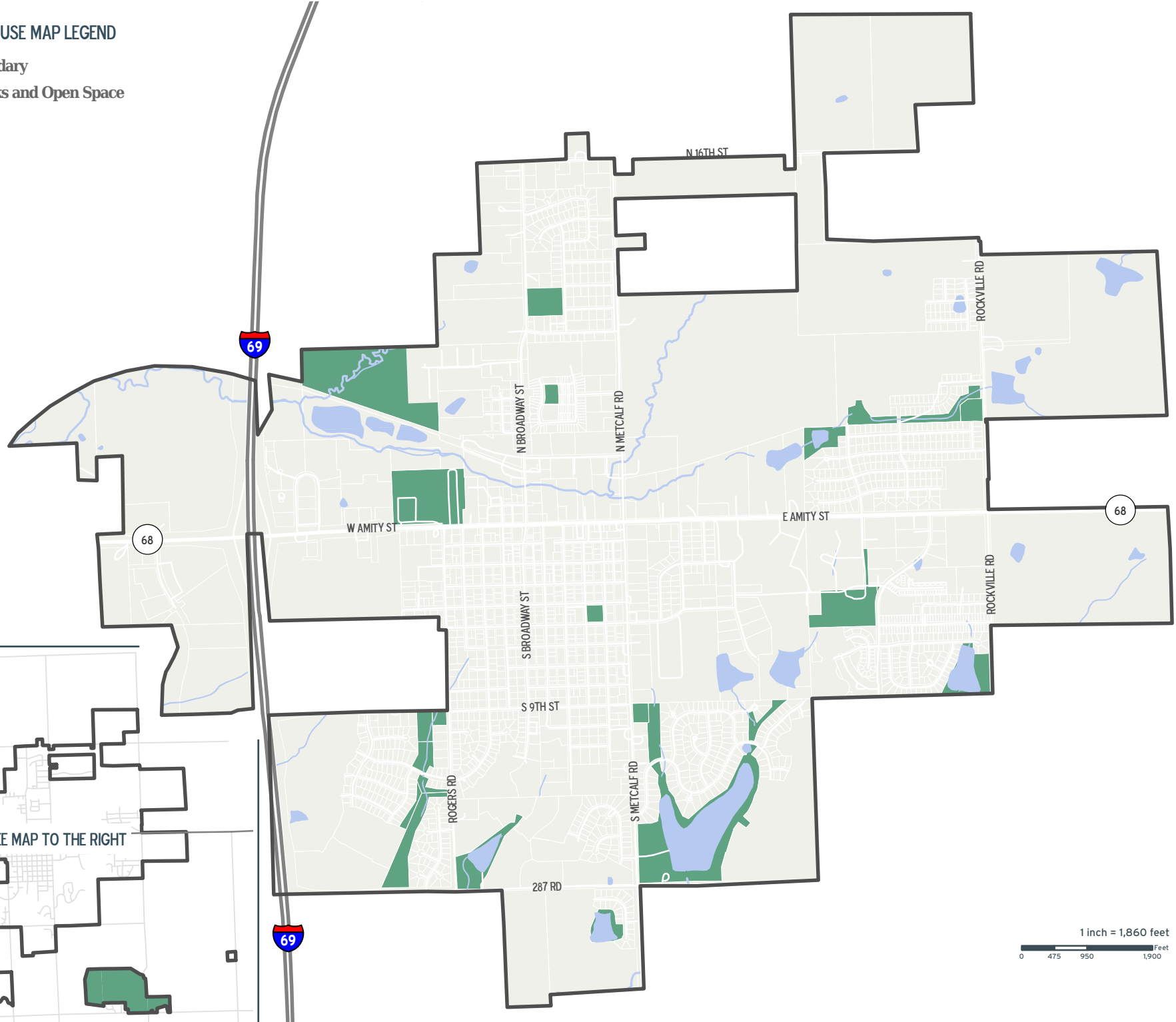
Parks and Open Space encompasses all parks, open space and recreational lands within Louisburg. The intent of this classification is to preserve such areas, enhance their functions as active centers for recreational activity, and to achieve a well-connected system of open spaces, trails and “green” infrastructure within the Louisburg community.

Environmentally-sensitive lands, cemeteries, floodplains, utility rights of way, and significant natural and topographic features are also categorized under this land use classification. While no new parks are proposed in this Comprehensive Plan, future park spaces and greenways may potentially be added as part of future residential subdivision developments.

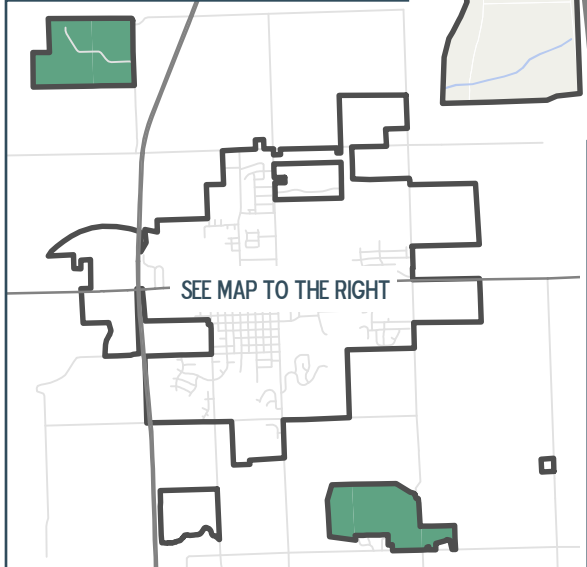


PROPOSED LAND USE MAP LEGEND

-  City Boundary
-  P/OS, Parks and Open Space



CITY-WIDE MAP



AGRICULTURAL LAND (AL)



Agricultural Land areas are located primarily in areas surrounding Single-Family Residential Neighborhoods to the north, east and south.

These areas are expected to remain in agricultural use for the time horizon of this Comprehensive Plan, given current market and infrastructure constraints. Agriculture, both general farming and restricted forms, such as pastures, gardening, kennel, or riding stables, nurseries and greenhouses, would be permitted uses. Single family dwellings on large lots related to agricultural use would also be permitted. This land use located outside of the municipal boundaries.





LAND USE CHANGES

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The Future Land Use Strategy and Land Use Map describes the distribution of general land use categories - Residential, Commercial, Employment Center, Open Space and Parks, and so on - that will set the framework for Louisburg's growth and revitalization.



The strategy also seeks to reposition Louisburg's land use pattern given current market and development conditions and other critical needs to diversify the community's tax base. Several changes in land use classifications are proposed in this Comprehensive Plan:

Amity Street Corridor Commercial to Multi-Family Residential

As described previously, land along the north side of Amity Street, from Berkley Street to Middle School/High School complex could be the location for a Multi-Family Residential Neighborhood land use classification. The area's existing land use is commercial-industrial. Multi-family development in this location, whether fourplexes, townhomes or apartments, would provide a suitable transitional land use from corridor commercial at the corner of Amity Street and Metcalf Road to the single-family Summerfield Farm residential subdivision on the east; residential use north of Amity Street would also be more compatible with the school complexes to the south.

Highway 69 Residential/ Commercial to Employment Center/ Business Park

Land areas in the quadrant northeast of the Highways 68-69 interchange and along Amity Street south of the commercial developments between U.S. Highway 69 and the edge of the Traditional Residential Neighborhoods to the east could be suitable locations for employment-generating uses – small-scaled light industrial, back office operations, and warehousing-type establishments, among others. These areas are optimal locations for such uses given their relative adjacency to the Highway 68/69 interchange, and their settings away from Louisburg's Downtown, traditional residential core, and the school complexes to the east; truck traffic would be largely limited to Amity Street. Any employment-generating/business park use on the south side of Amity Street would require landscaping and buffering treatments to adjacent single-family neighborhoods.

CITY FORM AND GROWTH

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A community grows and develops mainly due to its geographical characteristics and locational advantages. How and where Louisburg grows in the future will depend upon the availability and condition of its infrastructure and transportation systems.



Louisburg began as a farm community in the middle of the 19th century; a significant transportation feature that encouraged its development was the Missouri, Kansas, and Texas Railroad (MKT Railroad) that traveled east-west through the area north of Amity Street. The railroad was built from 1869-1871 and a depot was added in Louisburg in 1871. Older sections of the City were platted in 1868 in anticipation of the growth that would be spurred by the railroad. With the advent of the automobile, Kansas Highway 68 and Metcalf Road became the main transportation corridors into the community, replacing the MKT Railroad, which ceased service operation towards the end of World War II. With the construction of U.S. Highway 69 and the growth of Kansas City metropolitan area, Louisburg changed from a rural farm town to a bedroom community – that is, people lived in Louisburg, but now commute elsewhere to work.

The City doubled in size during the 1990s and early 2000s with the development of several new residential subdivisions; residential growth stalled during the Great Recession of 2008, but is starting to pick up again, although at a slower pace. Louisburg’s current population is estimated at 4,500 and MARC has projected a doubling of the City’s population by 2040. This may be unlikely given current housing trends.

Growth Strategies

While City growth in the residential sector will occur at a slower pace, opportunities exist to manage growth that enhances local quality of life and maintains the community’s desired small-town character.

- **Infill Development.** Many Louisburg stakeholders have asked how the community should promote infill growth as greenfield development occurs at the City’s edge. While newer development can be built less expensively on the greenfields surrounding the City, infill development can occur in and around the Downtown core given its proximity to the U.S. Highway 69 and the Amity Street interchange. Many people desire to live in a small town with a traditional street grid network as found in Louisburg’s Traditional Residential Neighborhood. An effective way to encourage infill growth is to modernize and update the street network in these blocks. This may mean adding sidewalks, streets curbs, storm sewers and increasing the size of the water lines. The City also may consider burying overhead power and telephone lines to make the area more attractive. While this will be very costly, the improvements will likely increase property values.



- New Development Areas.** The areas that will experience new development over the time horizon of this Comprehensive Plan will likely be those that have the quickest travel times to the U.S. Highway 69 and Kansas Highway 68 interchange. Most residential development will occur east of U.S. Highway 69 and east and north of Amity Street. Residential build-out of existing subdivisions should be a priority for the City administration. However, potential residential development could occur in other areas reserved for residential development over the following time frame based on current or projected construction permit activity summarized in the table below. The current level of single family residential construction activity is 50 permits in the past year. Limited residential development will occur west of U.S. Highway 69 due to the poorly-connected roadway network and the floodplain areas of North and South Wea Creek. Some

development will occur along Amity Street west of U.S. Highway 69 and may proceed north on Spring Valley Road. Business park and employment-generating uses are envisioned in this area. There is a bridge crossing of U.S. Highway 69 north of Amity Street at North 16th Street. However, from this location, it is not easy to reach the U.S. Highway 69 and Kansas Highway 68 interchange since one would have to travel west to Spring Valley Road or east to Broadway Street to connect to an interchange. There are bridge crossings of U.S. Highway 69 south of Amity Street at South 9th and South 16th Streets. Some development may occur west of U.S. Highway 69 at these locations, again mainly envisioned as business park

- Transportation and Interchanges.** The 5-County Study suggested that installing an interchange at South 16th Street could alleviate future traffic problems on Amity Street. However, there are no current plans to pursue this. An interchange at this location or another location in this area should be seriously considered as an alternative to widening Amity Street. This could potentially spur growth to the south.

Residential Land Build-Out			
Residential Land Available	Potential Buildable Lots	Build-Out Time Frame	Impacts
600 acres (Single-Family Detached Residential Land Use)	2,067 lots/units	@50 permits per year: 41 years @100 permits per years: 20 years @150 permits per year: 14 years	<ul style="list-style-type: none"> Elementary School students generated per unit: 423 Middle School students generated per unit: 225 High School students generated per unit: 213



- **Annexation and Growth Boundary.** The City should monitor development on the City’s periphery that occurs in unincorporated areas. The City can object to development on its outskirts but not veto it. Development in these areas may subsequently be annexed into the City; therefore, the City should review all new developments within, say two miles of City limits with an understanding that it will eventually be annexed into the City. It should be noted that this Comprehensive Plan continues the previous annexation policy of the 2000 Comprehensive Plan – in that annexation should be considered only in areas where private sector development or development pressures are occurring over this Plan’s time horizon. The City should continue to work closely with Miami County in reviewing development that occurs on the outskirts of the City limits.

